



Village of Morton Grove  
**PLAN COMMISSION MEETING**

Tuesday, September 16, 2025 - 7:00 P.M.  
Flickinger Municipal Center, 6101 Capulina Avenue, Morton Grove, IL 60053

**AGENDA**

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I. CALL TO ORDER

II. APPROVAL OF MINUTES: July 29, 2025

III. PUBLIC HEARINGS:

CASE: PC 25-07

PETITION: Request for a request for approval of a Preliminary Plat of Subdivision, in accordance with Chapter 12-8 of the Morton Grove Municipal Code, and Special Use Permits for a 60-unit mixed-use development with ground floor commercial space in a C/R Commercial/Residential District (12-4-3) with variations for rear yard impermeable coverage (12-2-5:B.3), setback for open accessory parking spaces and balconies (12-2-6:G), facade transparency (12-5-7:A.3.k), dwelling units per acre (12-5-7:C), residential unit location (12-5-7:D.1), parkway trees (12-11-2:B.4), and parking lot screening abutting private property (12-11-3:B.2) for the property commonly known as 8500-8550 Lehigh Avenue in Morton Grove, Illinois. The applicant is 8500 MG, LLC.

IV. CLOSE MEETING

Note that all persons are welcome to attend the public meeting in-person as regularly scheduled. Comments relating to this case may also be submitted no later than 12:00 p.m. on Tuesday, September 16, 2025, to [bnolin@mortongroveil.org](mailto:bnolin@mortongroveil.org). All comments received in relation to this case will be read at the public hearing for consideration by the Plan Commission.



MINUTES OF THE JULY 29, 2025  
SPECIAL MEETING OF THE MORTON GROVE PLAN COMMISSION  
MORTON GROVE VILLAGE HALL, 6101 CAPULINA AVENUE, MORTON GROVE, IL 60053

Pursuant to proper notice in accordance with the Open Meetings Act, the special meeting of the Plan Commission was called to order at 7:00 p.m. by Chairman Chris Kintner. Secretary Kirchner called the roll.

Commissioners Present: Dorgan, Liston, Mohr, Stein, and Kintner

Commissioners Absent: Hussaini without notice, Mohr with notice

Village Staff Present: Brandon Nolin, AICP, Community Development Administrator;  
Anne Kirchner, Planner/Zoning Administrator and Secretary;

Trustees Present: Mayor Witko, Minx, Thill and White

Chairman Kintner described the procedures for the meeting. The Village will present the case and the Plan Commission may ask questions of the applicant. Then, anyone from the audience will be allowed to provide comment to the Plan Commission on the case. The Commission's decision is a recommendation to the Village Board. Chairman Kintner acknowledged there were 5 Commissioners present.

A motion to approve the minutes of the June 17, 2025 meeting was made by Commissioner Liston and was seconded by Commissioner Dorgan.

Commissioner Dorgan	voting	aye
Commissioner Liston	voting	aye
Commissioner Ingram	voting	abstain
Commissioner Stein	voting	aye
Chairman Kintner	voting	aye

Motion passed (4-0).

CASE: PC 25-06, continued

APPLICANT: Bridge Industrial

LOCATION: 8125-45 River Drive and 8120-40 Lehigh Avenue  
Morton Grove, Illinois 60053

PETITION: Request for approval of a Preliminary Plat of Subdivision with associated waivers in accordance with Chapter 12-8 of the Morton Grove Municipal Code, and a Special Use Permit for redevelopment to establish warehousing, distribution centers, and light manufacturing uses at the properties commonly known as 8125-45 River Drive and 8120-40 Lehigh Avenue, Morton Grove, Illinois (PIN 10-20-303-001-000; 10-20-303-002-000), all within M-O/R Office/Research Manufacturing District per Section 12-4-4:E, with select waivers regarding setbacks, landscaping, signage, and parking located in a street side yard per Sections 12-2-6 and 12-4-4 and Chapters

10-10 and 12-11; and approval of a Preliminary and Final Plat of Subdivision in accordance with Chapter 12-8. The applicant is Midwest RE Acquisitions, LLC which is an entity of Bridge Industrial.

Mr. Nolin stated in the case of PC 25-06, the applicant Midwest RE Acquisitions, LLC (which is an entity of Bridge Industrial) is requesting approval of a Special Use Permit to demolish the existing office buildings at the North Grove Corporate Park and establish warehousing, distribution centers, and light manufacturing uses at the property. The property is zoned MOR and all surrounding properties are zoned M-2 General Manufacturing.

On June 17, 2025, Bridge Industrial presented a 227,600-square-foot speculative single-story industrial building including 35 truck loading berths, approximately 212 off-street parking spaces, underground stormwater detention, and various site improvements. In response to feedback received at the Plan Commission meeting, Bridge Industrial revised the proposed concept to include a smaller 196,333-square-foot building with 28 loading berths and surface stormwater detention.

- While the proposed concept has been reduced in size, aspects regarding landscaping, construction and façade materials, and signage remain very similar.
- The applicant revised the proposed lighting to address staff concerns regarding low light levels in some areas.
- The addition of a detention area on the west side of the property provides a greater buffer to properties to the west, but eliminates the potential for access drives. Truck access is now solely from the east-west segment of River Drive on the north side of the subject property.

In reviewing the revised application against previous Plan Commission comments, Staff has concerns with the proposed truck volume and number of loading berths. This includes concerns regarding heavy truck use along Lehigh Avenue and potential impacts to bike and pedestrian routes and key utility infrastructure.

Staff requests that the Plan Commission table the current application, so that additional negotiations can occur with the applicant including potential revisions to the uses being requested for approval, number of truck bays, and/or truck volumes permitted to be generated at the subject property.

Chairman Kintner noted the application needs to be revised due to changes being made in the plan. He noted that prospective tenants will have different zoning requirements and asked how that would be addressed by the developer instead of by a future tenant. Mr. Nolin said the applied zoning is currently based on a highest use scenario shown in the traffic and parking study. Chairman Kintner said the modeling is based on a similar building but not on the site.

Condition number 7 was discussed, it does not list light manufacturing as a use, the intent is to limit the traffic volume. Condition number 11 restricts northbound truck traffic on Lehigh, it was asked how that would be controlled.

Commissioner Liston asked if the traffic study was updated and if the Village had made a determination regarding the Cook County class 6B application. The study has been updated to reflect Metro on Main and the proposed TOD development to the north, and the review of the application has not been completed.

The applicants were sworn in by Secretary Kirchner. Chairman Kintner asked if the applicant understood that the case is tabled. It was acknowledged and noted they would run through the changes to the plan and ask for feedback.

Curt Pascoe, Executive Vice President of Development for Bridge Industrial introduced the team for the site, and provided the group's history with examples of their other work. He noted the site held a heavy manufacturing use from the 1940's to the 1980's when it was redeveloped into the 2 current office buildings in an office park. The offices are predominately vacant and the land is a brownfield site requiring environmental clean-up.

Changes made to the plan involve a 15% reduction in building size, and a larger, 127 feet setback on the west side of the property along River Drive. A stormwater detention area on the west side increased the greenspace from 18% to

30% and loading berths were decreased from 34 to 28 berths. Modifications also allow for the preservation of more trees.

Specific to landscaping, more trees are proposed for the parking area landscape islands. Total trees, new and preserved, now total 264. It is the intent to address all conditions in the Staff report and make modifications.

Chairman Kintner thanked the applicant for their cooperation and responses. Commissioner Dorgan said if this development would generate sales tax to the Village. Certain tenants would, not all warehousing would.

Commissioner Ingram asked why they are asking for additional height without knowing what tenant would be occupying the space. Mr. Pascoe said the majority of these types of building have a market standard of this height.

Commissioner Stein asked if the glass at the southeast and southwest corners is spandrel glass and noted the large glass areas that do not serve a function and may perpetuate a bird strike problem.

Commissioner Ingram asked if the detention pond has been reviewed by the Village engineer. Discussion ensued regarding the drawdown rate. Tom Szafranski of Kimley Horn said the release is as quickly as allowed by MWRD. The pond is meant to be a wetland, landscaped area until it is used for detention. The pond connects to the storm sewer system.

Commissioner Ingram said he is concerned with truck traffic and the amount of 18-wheel trucks that could be on site. As the tenant is unknown, how can we know those numbers? Mr. Pascoe said they would like feedback from the Commissioners as to what uses would be allowed by right or by administrative approval. He does not foresee the space to be solely used by large trucks, but it is an unknown.

Chairman Kintner noted the KOLA traffic study is based on numbers generated by a similar facility, what and where is that facility? What were the tenants and hours of operation? That information is not available and will be provided at the next meeting.

Chairman Kintner said the passenger vehicle trips seem quite high. Mr. Pascoe noted the study is number of trips, with 200 required parking spaces, there could be 800 trips per day. He further asked about level of service on Lehigh which is rated B, and D grades for level of service on River. The levels can only be worse with the development.

Louis Aboona with KOLA, covered the grades at the signaled intersections on Oakton. IDOT controls the signals to allow for reasonable levels of service, there are many variables including the railroad tracks. It would become the burden of the Village to have IDOT change the signaling to address back-ups on Lehigh and River. Mr. Aboona said their analysis accounted for the added traffic and did not find the level of service troubling.

Chairman Kintner said that the Commission must consider the real-life impacts of the traffic on the surrounding buildings. Mr. Pascoe said they are tying an objective number of truck trips to permitted uses, and the Commission seems to want that number to be lower. He asked for guidance from the Commission.

Chairman Kintner said that is not the responsibility of the Commission, we need to look at proposals that are in the best interest of the Village.

Chairman Kintner asked about possible solar panels and the permit process. The Village is adopting standards for solar installations.

Mr. Mike Tracy said the developer did reach out to him prior to the new design. His opinion is this will not be a manufacturing facility, it will be logistic facility and will not generate any sales tax. He is concerned with semi-truck traffic.

Mr. Nolin read the first paragraph of a letter from Mr. Tracy, the entire letter is in the public record (Attachment A).

The class 6B application is not under consideration by the Plan Commission, it is reviewed by the Village Board of Trustees.

One other letter was read from Jeff Ware, COO of Catering by Michaels (Attachment B).

Commissioner Liston moved to table the agenda item to a future date, the motion was seconded by Commissioner Dorgan. Chairman Kintner called for the vote.

Commissioner Dorgan	voting	aye
Commissioner Ingram	voting	aye
Commissioner Liston	voting	aye
Commissioner Stein	voting	aye
Chairman Kintner	voting	aye

Motion passed (5-0)

There was no further business.

Commissioner Liston moved to adjourn the meeting by acclamation. The motion was seconded by Chairman Kintner.

The motion to adjourn the meeting was approved unanimously pursuant to a voice vote at 8:13 p.m.

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Minutes by: Anne Kirchner

**Mr. Brandon Nolin  
Community Development Administrator Dept. of Community & Economic Development  
6101 Capulina Ave.  
Morton Grove, IL 60053**

**RE: Formal Opposition and Concerns Regarding the Proposed Development at 8125-45 River Drive**

**Mr. Nolin and Concerned Parties:**

This letter formally expresses Michael A. Tracy's and Design Installation Systems, Inc.'s (DIS) significant and profound concerns regarding the proposed redevelopment located at 8125-45 River Drive in Morton Grove. As a business that has been a cornerstone of the Morton Grove community for over 35 years, operating at 8110 and 8120 River Drive, we are deeply invested in the well-being and orderly development of this subdivision. Our position is one of strong opposition to the current proposal due to its detrimental aesthetic, operational, financial, and legal implications for the immediate area and the broader Village.

The proposed development, as currently designed, fundamentally deviates from the established character and functionality of the M-O/R Office/Research Manufacturing District. It threatens to introduce unprecedented levels of traffic and nuisance, diminish property values, and offers no demonstrable benefit to the Village, all while failing to meet critical land use standards. We believe these issues must be thoroughly addressed for this development to be successful for all interested parties.

**I. Detailed Concerns Regarding Development Plan and Design**

- **A. Site Design and Operational Impact:**
  - **Excessive and Misplaced Loading Docks:** The proposal includes 28 new loading docks, representing a staggering 280% increase over the current 10-12 docks servicing the entire subdivision. Critically, these docks are positioned to heavily impact River Drive and Park Avenue. This dramatic increase in loading docks, combined with their proposed location, indicates a fundamental shift in the site's intended operational intensity, far beyond what the existing infrastructure and surrounding businesses can reasonably accommodate. This scale of operation signals a high-traffic, high-nuisance activity that is inherently incompatible with the M-O/R district's intent for "non-impactful" uses. The sheer volume of trucks will overwhelm local roads, leading to constant congestion and safety issues. Despite assurances from Bridge that trucks will not idle or remain in the vicinity after pickups or drops, realistically, they have zero control over the operators once they leave the development's property.
  - **Proposed Building Rotation for Optimal Traffic Flow:** We strongly advocate for rotating the building by one axis to the east, which would strategically place the loading docks onto the Lehigh Avenue elevation. This single change would significantly mitigate the impact of semi-truck congestion on River Drive and Park Avenue, alleviating trailer parking and traffic concerns for existing businesses. The proposed design removes the Lehigh access point, forcing all traffic onto River and Park Avenues. The fact that such a logical and beneficial alternative—utilizing Lehigh for docks—was not incorporated or even considered by the applicant demonstrates a fundamental disregard for the existing community's operational

harmony and safety. This indicates a "wedged" development approach rather than an integrated one, forcing the community to bear the burden of the design's shortcomings. In the event this application is approved, we will continue to advocate for the rotation of the building to situate the docks onto the east. If one of the Village's primary concerns is having 28 "unsightly" docks located on Lehigh, then you can certainly understand our opposition to having the docks facing our development.

- **Non-conforming Side Yard Setbacks:** Side yard setbacks for the property should follow as closely as possible to match the footprint of the existing two office buildings' current setbacks to ensure uniformity and aesthetic appropriateness. This concern, coupled with the need for "substantial waivers and variances" for the proposed development, reinforces the argument that the proposed development is not suitable for the site and is being "wedged" in, rather than designed to fit harmoniously. This is not just about aesthetics; it is about the physical manifestation of an unsuitable use being forced into a limited space, which often leads to operational compromises like narrowed truck lots and increased congestion along Park, River, and Lehigh. The copious requests for waivers and exemptions speaks to the inappropriateness of the proposed development.
- **B. Building Aesthetics and Community Integration:**
  - **Unstated Building Height and Sunlight Implications:** The proposed building's height is currently 42 feet, yet it is understood that 40 feet is the maximum allowed for this location. We believe the height should be reduced to approximately 30 feet, consistent with the Grainger location, which is currently the tallest structure in the area. Furthermore, rooftop mechanicals and penthouses must be included in the 40-foot height calculation to prevent exceeding the limit. The proposed height also raises significant concerns about sunlight obstruction for surrounding properties. The lack of a stated height, combined with the potential for exceeding limits via uncalculated mechanicals, suggests an attempt to maximize building volume without full transparency or consideration for the immediate environmental impact on neighbors. The explicit mention of sunlight issues points to a direct negative impact on adjacent properties, affecting quality of life or even solar energy potential. Once differing elevation heights and mechanicals are considered this will easily be a 5-story structure.
  - **Lack of Façade Design Uniformity:** To ensure uniformity and aesthetic appropriateness, the façade design must remain the same and be consistent along all four elevations, mirroring all existing properties in the subdivision. Inconsistent façade design is often a cost-saving measure that prioritizes budget over community aesthetic integration, leading to a visually disjointed development that detracts from the overall subdivision character. This implies that current plans may only show a "front" façade or that different elevations are proposed with varying levels of architectural detail, undermining the established aesthetic.
  - **Insufficient Landscaping and Non-compliance with New Tree Ordinance:** Unresolved landscaping items persist in the plans. Strict compliance with Morton Grove's recently adopted new tree ordinance is imperative for this completely new development. Furthermore, additional tree plantings are required to adequately soften the proposed building's increased bulk and height. The failure to meet landscaping requirements and the new tree ordinance indicates a disregard for environmental responsibility and community green space, exacerbating the visual impact of an already oversized structure. The recent adoption of a new tree ordinance signifies the Village's commitment to environmental quality and

community aesthetics, and the applicant's apparent non-compliance suggests a lack of alignment with the Village's current development priorities. Significant modifications have also been made to the original application with the relocation of the retention basin to the west. The relocation of the basin's impact on the landscape plan will now require it to be revisited by the appearance commission for their approval.

- **Disproportionate Increase in Square Footage:** The proposed increase in square footage from approximately 147,000 sq ft to 200,000 sq ft (an additional 53,000 sq ft) raises serious questions about whether this proposal maintains the current aesthetic and spirit of the existing subdivision buildings. This substantial increase in building mass, combined with other design deficiencies, suggests an overdevelopment of the site that fundamentally alters the established scale and character of the commercial subdivision. This approx. 40% increase is not merely an expansion but a significant intensification of land use.
- **Concerns with New Retainage Layout**  
The reconfiguration of the drainage basin needs to be analyzed in a deeper capacity. Further engineering and studies are required. Drainage accommodations and depths of the basin need to be flushed out in more detail to understand its global impact on the developer's application. The depth of the basin is important to understand as it has irrefutable aesthetic implications to the neighborhood and will create an illusion of a 5-story building looking more like a 6-story building due to the lower grading. Again, this should be revisited by the appearance commission as it alters the original application.

## **II. Fundamental Objections to Special Use Request and Broader Community Impact**

Morton Grove, as a home rule municipality, has broad powers to govern its own affairs. However, decisions regarding zoning and special use permits must align with principles of public good and avoid arbitrary and unreasonable outcomes.

- **A. Inconsistency with M-O/R District Intent:**
  - **Proposed Uses are Incompatible:** The M-O/R Office/Research Manufacturing District is specifically intended for "office and research uses along with non-impactful light industrial, technology 'start-ups' and other similar, complementary uses". The proposed uses – warehousing, distribution centers, and light manufacturing – are explicitly *not* intended uses for this district, particularly those generating "heavy truck traffic volumes". The M-O/R district's definition explicitly aims to prevent nuisances and restrict heavy truck traffic. The proposed uses are inherently high-nuisance and high-traffic, creating a direct contradiction. Approving a special use permit for uses that are explicitly not intended for the district, and which contradicts its core purpose of nuisance prevention, would effectively render the M-O/R zoning meaningless. This would undermine the Village's comprehensive plan and set a precedent for future developments to disregard zoning regulations.
  - **Lack of "Low Nuisance" Determination and Irrelevant Traffic Study:** There has been no determination that the proposed uses are "low nuisance" or will not generate "heavy truck traffic". Furthermore, the applicant's traffic study relied on "Land Use Code 110 (General Light Industrial)," which is *not* one of the three uses requested, rendering its relevance highly questionable and making it impossible to accurately assess nuisance and traffic impact. The applicant's failure to provide specific user information and reliance on an irrelevant traffic study indicates a deliberate attempt to secure a "blanket approval" without transparently demonstrating the actual impact or suitability of the proposed operations. Each of the three proposed uses has vastly different impacts on traffic, noise, and overall nuisance, making any assessment speculative without specific user information.

- **B. Severe Traffic and Nuisance Implications:**
  - **Elimination of Critical Lehigh Access Point:** The proposed development removes the Lehigh access point, which currently serves as a vital entry/exit point for the commercial subdivision. This forces substantially all truck traffic onto Park Avenue. This design decision actively creates a traffic bottleneck and safety hazard, demonstrating a fundamental disregard for existing traffic patterns and the operational needs of surrounding businesses. This shifts the burden of the development's operational intensity onto existing public infrastructure and businesses, creating a systemic safety risk and operational impediment.
  - **Unsafe Turning Radii and Inevitable Site/Roadway Congestion:** Turning radii exhibits demonstrate that WB-65 (53-foot typical over-the-road trucks) will require multi-point turning movements *within* the proposed development, leading to collisions and site congestion. Furthermore, non-linear turning paths are required to avoid collisions with vehicles on River or Park Avenues, exacerbated by 28 loading docks encroaching on the River Road setback and a narrowed truck loading lot. The proposed layout is inherently unsafe and impractical for the scale of operations it aims to support, guaranteeing daily congestion and a heightened risk of accidents on public roads. The "correspondingly narrowed" truck loading lot and setback encroachment indicate that the site is simply too small for the proposed scale of operations, leading to unavoidable operational bottlenecks that will spill onto public roadways, impacting the safety and accessibility of the entire subdivision. In the event this application is approved, we will continue to advocate for the rotation of the building to situate the docks onto the east. If one of the Village's primary concerns is having 28 "unsightly" docks on Lehigh, then you can certainly understand our opposition to having the docks facing our development.
  - **Inadequate Roadway Width and Lack of Truck Queuing Space:** Park Avenue (and River Road) are only 50 feet wide, with no area for truck queuing. This will inevitably lead to access issues for both the proposed development and existing businesses, as trucks will queue on public roads. The 50-foot width is insufficient for the volume of 53-foot trucks needing to access 28 docks, especially with multi-point turns. Without dedicated queuing space, trucks will inevitably queue on Park Avenue, blocking traffic and impeding access to other businesses. This creates a direct and severe operational nuisance for all businesses on Park Avenue and River Drive, impacting emergency vehicle access, customer access, and general traffic flow, directly violating the M-O/R district's intent to avoid nuisances.
- **C. Absence of Demonstrated Benefit to the Village:**
  - **Significant Financial Detriment from 6B Tax Incentive:** The applicant claims the proposed uses will stabilize the Village's tax base. However, they are simultaneously seeking a 6B tax incentive, which would assess the property at 10% of market value for years 1-10 (versus the standard 25%). This translates to an estimated **\$720,000 loss per year, totaling \$7.2 million over 10 years**, with reduced rates continuing for years 11 and 12. The applicant's claim of stabilizing the tax base is a direct contradiction to their request for a substantial tax incentive. This represents a significant net financial loss for the Village, shifting the tax burden elsewhere and undermining the very benefit they claim to offer. This financial analysis reveals a critical contradiction in the applicant's stated benefits, as the Village would be subsidizing a development that generates significant negative externalities while simultaneously losing substantial tax revenue. Furthermore, with no signed lease or committed manufacturing tenant in place, it is inappropriate for a 6B incentive to be under consideration given the requirements of the 6B application rules from the Village and County. Please see the attached exhibit of Cook

- County's requirements for 6B status. The addition of 28 docks also indicates that manufacturing is not the end goal at this development.
- **Unsubstantiated Claims of Employment Diversification and Job Creation:** The applicant claims the development will diversify employment and create new jobs. However, no user(s) have been identified, there are no job creation requirements in the application, and the trend in warehousing and distribution is towards automation, not job growth (e.g., Amazon's deployment of over 750,000 robots). The claims of employment benefits are speculative and unsupported, likely serving as a rhetorical justification rather than a concrete commitment. The explicit mention of automation trends directly contradicts the premise of significant job creation, suggesting that the "employment benefit" is a hollow promise.
  - **Unproven Opportunities for Existing Village Businesses:** The applicant asserts new business activity will create opportunities for existing Village businesses. Again, without identified users, the scale and scope of this "new business activity" cannot be demonstrated, rendering this claim equally speculative. This claim is a vague generalization without any concrete basis, failing to offer a tangible benefit to the existing business community.

### III. Failure to Satisfy Established LaSalle/Sinclair Factors

Decisions regarding zoning and special use permits must align with principles of public good and avoid arbitrary and unreasonable outcomes, as guided by the LaSalle/Sinclair factors. The proposed development fails to meet these critical tests:

- **Inconsistency with Existing Uses and Zoning of Nearby Properties:** The proposed development, with its 28 loading docks and high traffic dependency, is fundamentally inconsistent with nearby properties characterized by long-term, low-traffic users and only ten total loading docks in the entire subdivision. This inconsistency demonstrates a clear departure from the established character and functionality of the area, creating a jarring and detrimental juxtaposition. The sheer numerical difference in loading docks highlights a radical shift in the type of industrial activity.
- **Demonstrable Diminution of Property Values and Harm to Public Welfare:** Higher traffic volume and congestion, coupled with increased nuisance (noise, vibration, potential odors), will inevitably make nearby properties less appealing to existing and potential users, thereby diminishing their inherent value and harming the health, safety, and general welfare of nearby business owners and the community. The increased traffic, noise, and potential for collisions are not abstract concerns; they are direct contributors to a diminished business environment, leading to a measurable decrease in property values. The Village's role is to protect the general welfare, and approving this development would actively cause harm to long-standing contributors to its tax base and community fabric.
- **Lack of Hardship Justification for the Applicant:** No hardship has been shown for the applicant. The subject property has been successfully utilized in its present use for over 30 years. The absence of hardship indicates that the applicant's proposal is driven by a desire for maximum profit or a specific, high-intensity use, rather than a necessity to redevelop an unviable property. If the property has been successfully utilized for decades, there is no inherent "hardship" compelling this specific redevelopment.
- **Unsuitability of Property for Proposed Use, Requiring Extensive Waivers:** The property is demonstrably unsuitable for the proposed use. To accommodate the development, substantial waivers and variances are required, indicating that the development is literally "wedged" into the site's footprint. The requirement for "substantial waivers" (e.g., setbacks, landscaping, signage, parking in street side yard) is a strong indicator that the proposed development does not conform to the established land use regulations. This is not a minor adjustment; it is a fundamental reshaping of the rules to

accommodate an unsuitable project. The "wedged" description reinforces the idea that the design is too large and too intensive for the available parcel, forcing compromises that negatively impact the surrounding area and requiring the Village to compromise its own standards.

- **Absence of Demonstrated Community Need for the Proposed Use:** No community need for the proposed use has been shown; the applicant cannot even identify a user. Granting a special use permit without a demonstrated community need or identified user is irresponsible and speculative, potentially leading to a development that serves no public purpose while creating significant private harm. A special use permit is typically granted when a specific use serves a demonstrated community need, and the applicant's inability to articulate this undermines the very premise of a special use request. There is currently approximately 1 million sf of similar warehousing space available located within 3000 ft of the proposed development. This shows the lack of a need for this specific type of warehousing/logistics development.
- **Insufficient Planning and Failure to Consider Viable Alternatives:** No alternative designs were considered. The building could be smaller, with fewer loading docks, and with less risk of traffic and safety impacts. Crucially, the Lehigh access for trucks could have been retained, completely eliminating harms to existing businesses. The applicant's failure to explore or adopt design alternatives that would mitigate negative impacts demonstrates a lack of responsible planning and a disregard for community well-being. The existence of clear, viable alternatives that would significantly reduce negative impacts but were not considered suggests a singular focus on maximizing the applicant's benefit at the community's expense. This exposes a critical flaw in the applicant's planning, implying that the proposal is not the result of thoughtful integration but rather a rigid plan forced upon an unsuitable site.

Based on the comprehensive concerns detailed above, Michael Tracy and Design Installation Systems, Inc. reiterate our strong opposition to the proposed development at 8125-45 River Drive as currently presented. The cumulative impact of incompatible uses, severe traffic and nuisance implications, significant financial detriment to the Village, and the failure to meet established land use principles renders this proposal unsuitable for approval.

We urge the Village of Morton Grove to carefully consider these critical issues, which directly impact the long-term health, safety, welfare, and economic vitality of our community and the integrity of its zoning ordinances. These concerns are not merely preferences but fundamental flaws that must be addressed for any development to be successful for all interested parties. We look forward to listening and contributing at both the appearance and zoning meetings, and we hope that all of these concerns are thoroughly answered and addressed at that time. We remain committed to engaging constructively to find a solution that respects the established character of our community and ensures a sustainable future for all businesses and residents but will also pursue all available legal remedies should the need arise.

Regards,  
Michael A. Tracy  
President  
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## **CLASS 6B ELIGIBILITY BULLETIN**

### ***Cook County Living Wage Ordinance***

Please be advised that every applicant for this incentive will be required to provide an affidavit to the Assessor's Office to confirm compliance with the Cook County Living Wage Ordinance. The Cook County Assessor will not grant any request for incentive classification until it receives the required affidavit.

### ***Incentive Benefits***

The Class 6b classification is designed to encourage industrial development throughout Cook County by offering a real estate tax incentive for the development of new industrial facilities, the rehabilitation of existing industrial structures, and the industrial reutilization of abandoned buildings. The goal of Class 6b is to attract new industry, stimulate expansion and retention of existing industry and increase employment opportunities.

Under the incentive provided by Class 6b, qualifying industrial real estate would be eligible for the Class 6b level of assessment from the date that new construction or substantial rehabilitation is completed and initially assessed or, in the case of abandoned property, from the date of substantial re-occupancy. Properties receiving Class 6b will be assessed at 10% of market value for the first 10 years, 15% in the 11th year and 20% in the 12th year. This constitutes a substantial reduction in the level of assessment and results in significant tax savings. In the absence of this incentive, industrial real estate would normally be assessed at 25% of its market value.

Where buildings or other structures qualify for the incentive as new construction or as abandoned property as defined below, the reduced level of assessment under Class 6b will apply to those structures in their entirety as well as to the land upon which they are situated. Where there is substantial rehabilitation of an existing structure which has not been abandoned, the reduced incentive level of assessment is applicable to the additional market value attributable to the rehabilitation, including qualified land related to the rehabilitation. ***(Please note that the additional value attributable to the rehabilitation for assessment purposes is likely to be lower than the actual amount spent on the rehabilitation.)*** Land qualifies when the rehabilitation adds vertical or horizontal square footage to the improvements. The amount of land eligible for the incentive shall be in such proportion as the square footage added by the rehabilitation bears to the total square footage of the improvements on the parcel.

## ***Eligibility Requirements***

Real estate is eligible for Class 6b status under the following conditions:

1. The real estate is used primarily for "industrial purposes".
2. There is either (a) new construction, (b) substantial rehabilitation, or (c) substantial re-occupancy of "abandoned" property.
3. An Eligibility Application and supporting documents have been timely filed with the Office of the Assessor according to deadlines as set forth in the "What Must Be Filed" and "Time for Filing" sections of this Bulletin.
4. The municipality in which such real estate is located (or the County Board, if the real estate is located in an unincorporated area) must, by lawful resolution or ordinance, expressly state that it supports and consents to the filing of a Class 6b Application and that it finds Class 6b necessary for development to occur on the subject property.

The following definitions, as set forth in the Cook County Real Property Assessment Classification Ordinance, pertain to the Class 6b incentive provision:

***Industrial purposes:*** "Any real estate used primarily in manufacturing ... or in the extraction or processing of raw materials unserviceable in their natural state to create new physical products or materials, or in the processing of materials for recycling, or in the transportation or storage of raw materials or finished or partially finished physical goods in the wholesale distribution of such materials or goods for sale or leasing."

***Manufacturing:*** "The material staging and production of goods used in procedures commonly regarded as manufacturing, processing, fabrication, or assembling which changes existing material into new shapes, new qualities, or new combinations and including research and development associated with the production of goods."

***Abandoned property:*** "Buildings and other structures that, after having been vacant and unused for at least 24 continuous months, are purchased for value by a purchaser in whom the seller has no direct financial interest." An exception to this definition shall be, "if the municipality or the Board of Commissioners, as the case may be, finds that special circumstances justify finding that the property is 'abandoned' for the purpose of Class 6b."

The finding of abandonment, along with the specification of the special circumstances, shall be included in the resolution or ordinance supporting and consenting to the incentive application. Notwithstanding the foregoing, special circumstances may not be determined to justify finding that a property is deemed "abandoned" where:

- A. There has been a purchase for value and the buildings and other structures have not been vacant and unused prior to such purchase; or
- B. There has been no purchase for value and the buildings and other structures have been vacant and unused for less than 24 continuous months.

If the ordinance or resolution containing a finding of "special circumstances" is that of a municipality, the approval of the County Board of Commissioners is required to validate such a finding that the property is deemed "abandoned" for purposes of the incentive, and a resolution to that effect shall be included with the eligibility application.

### ***What Must Be Filed***

An applicant seeking the reclassification of real estate to Class 6b is required to file a "Class 6b Eligibility Application" with the Office of the Assessor. At the time of filing the application, a filing fee of \$500.00 must be paid. In addition, an applicant may submit, at the same time, a certified copy of an ordinance or resolution adopted by the municipality in which the real estate is located (or the County Board, if the real estate is located in an unincorporated area) expressly stating that it supports and consents to the filing of a Class 6b Application and that it finds Class 6b necessary for development to occur on the subject property. If the resolution is not filed at the time the Eligibility Application is submitted to the Assessor, the applicant must file, at that time, a letter from the municipality or the County Board confirming that a resolution or ordinance supporting the incentive has been requested. If the applicant is seeking to apply based on the reoccupation of abandoned property and will be seeking a finding of "special circumstances" from the municipality, in addition to obtaining a letter from the municipality confirming that a resolution or ordinance supporting the incentive has been requested, the applicant must also file a letter from the County Board confirming that a resolution validating a municipal finding of special circumstances has been requested.

Should the municipality or the County Board, at a later date, deny the applicant's request for a resolution or ordinance, whether or not construction or re-occupancy has begun, the applicant will be deemed ineligible for reclassification to Class 6b. Any information that is not known or any supporting documents that are not available at the time of the initial filing must be submitted as a supplement to the Application.

After the construction or re-occupancy has taken place, an applicant must also file an "Incentives Appeal Form" requesting that the real estate be reclassified to Class 6b. At the time of filing the appeal, an appeal fee of \$100.00 must be paid. If a resolution from the municipality where the property is located, or the Cook County Board of Commissioners if located in an unincorporated area, was not filed with the Eligibility Application, the applicant must file a certified copy of the resolution or ordinance supporting the incentive at this time. No final action on a request for reclassification to Class 6b will be taken until an Appeal and an Eligibility Application, along with the required documentation as described therein, are completed and filed with the Office of the Assessor.

In addition, during the term of the incentive, the Class 6b recipient must file a triennial affidavit attesting to the use of the property and the number of workers employed at the Class 6b site. The Assessor will mail Class 6b recipients the affidavit forms at the time of their triennial reassessments. The affidavit must be signed, notarized and returned to the Assessor within three weeks. Failure to file the triennial affidavits within that time will result in the loss of the incentive.

### ***Time for Filing***

The Eligibility Application along with the appropriate resolution or letter confirming that a resolution has been requested *must be filed* with the Assessor *prior to*, but no earlier than one year before, *commencement of new construction* (excluding demolition, if any) *or substantial rehabilitation*. With respect to abandoned property, the eligibility application must be made to the Assessor ***prior to the commencement of the reoccupation of the vacant and unused property.***

Where reoccupation of "abandoned" property and subsequent substantial rehabilitation is planned, a single Eligibility Application and resolution, ordinance, or letter confirming that a resolution has been requested, may be filed for both situations, provided that the Application is filed prior to the commencement of reoccupation and such rehabilitation.

To finalize the classification change, a "Real Estate Assessed Valuation Appeal" must be filed after the construction or re-occupancy has taken place. In instances where a certified copy of an ordinance or resolution expressly stating that the municipality or County Board supports and consents to filing of a Class 6b Application has not yet been filed, it must be filed at this time. For the purpose of certifying final assessments on a timely basis to the Board of Appeals, deadlines for filing Appeals are established on a township basis. Check with the Office of the Assessor to determine when the deadline occurs for a particular township.

The 6b classification may be renewed during the last year in which a property is entitled to a 10% assessment level or when the incentive is still applied at the 15% or 20% assessment level, by filing a renewal application and a certified copy of a resolution or ordinance adopted by the municipality in which the real estate is located, or by the County Board, if located in an unincorporated area of Cook County, expressly stating that it supports and consents to the renewal of the Class 6b incentive and that it has determined that the industrial use of the property is necessary and beneficial to the local economy. The notice of intent to request renewal will be forwarded by the Assessor's Office to the Cook County Board. The owners must notify the Assessor's Office of their intent to request renewal at the time they request a resolution or ordinance agreeing to the renewal from the municipality or County Board. The number of renewal period requests is not limited.

**Questions regarding Class 6b may be directed to the Development Incentives Department of the Office of the Cook County Assessor, Room 301, 118 North Clark Street, Chicago, Illinois 60602, (312) 603-7529.**

## Proposed Development at 8125-45 River Drive


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**From** Jeffrey Ware <jware@cateringbymichaels.com>

**Date** Tue 7/29/2025 1:14 PM

**To** CommDev <commdev@mortongroveil.org>

**Cc** Lisa Ware <lware@cateringbymichaels.com>

 1 attachment (163 KB)

Michael A Tracy and DIS' Comments Regarding Bridge Development.pdf;

Hi Brandon,

I know when we spoke on the phone you told me the meeting regarding the proposed development at River/Lehigh/Park was being rescheduled and new notices would be sent out. We never received a new notice and just learned from a neighbor the meeting is tonight.

At this point we are unable to prepare a formal letter or attend the meeting this evening.

We would like to voice our concerns regarding the proposed development, especially as it relates to the requested 6B incentives.

On behalf of Lisa Ware, Catering by Michaels, MG Park Plaza, and Gator Holdings we agree with all of the comments and concerns addressed in the attached letter submitted by Michael Tracy and would like you to please notify the commission that the businesses we own are opposed to the development as planned.

We are not NIMBYs and not opposed to new development, especially if it were going to be beneficial to the village finances. Given that this development is being built on spec the commission and council cannot appropriately determine the need for or potential future benefit of providing a 6B incentive.



**Jeff Ware**

Chief Operating Officer

[cateringbymichaels.com](http://cateringbymichaels.com)

(847) 213-5012





To: Chairperson Kintner and Members of the Plan Commission

From: Brandon Nolin, AICP, Community Development Administrator  
Anne Ryder Kirchner, Planner/Zoning Administrator

Date: September 9, 2025

Re: Plan Commission Case PC 25-07

Request for a request for approval of a Preliminary Plat of Subdivision, in accordance with Chapter 12-8 of the Morton Grove Municipal Code, and Special Use Permits for a 60-unit mixed-use development with ground floor commercial space in a C/R Commercial/Residential District (12-4-3) with variations for rear yard impermeable coverage (12-2-5:B.3), setback for open accessory parking spaces and balconies (12-2-6:G), facade transparency (12-5-7:A.3.k), dwelling units per acre (12-5-7:C), residential unit location (12-5-7:D.1), parkway trees (12-11-2:B.4), and parking lot screening abutting private property (12-11-3:B.2) for the property commonly known as 8500-8550 Lehigh Avenue in Morton Grove, Illinois. The applicant is 8500 MG, LLC.

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## STAFF REPORT

### Public Notice

The Village provided Public Notice for the September 16, 2025, Plan Commission public hearing for PC 25-07 in accordance with the Unified Development Code. The Morton Grove Champion published a public notice on August 28, 2025. The Village notified surrounding property owners via mail and placed a public notice sign on the subject property on August 29, 2025.

### Application Summary

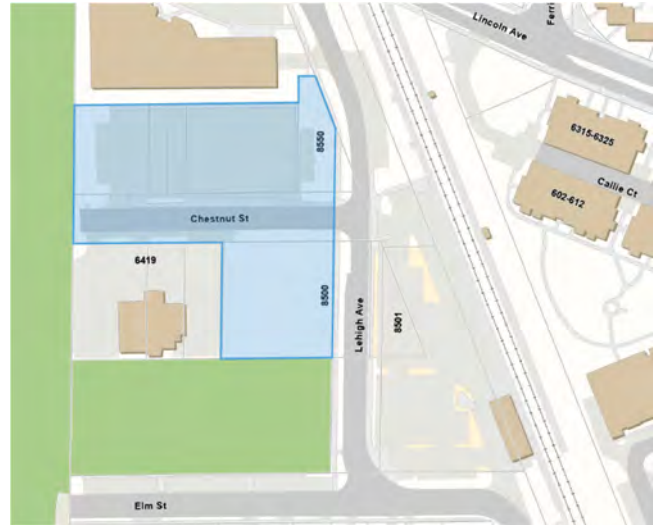
8500 MG, LLC ("applicant") submitted complete Vacation, Subdivision, and Special Use Applications to the Department of Community and Economic Development requesting approval of a vacation of a portion of Chestnut Street right of way, a subdivision and consolidation of vacated and Village-owned property, and Special Use Permits to allow the construction of a 57-unit mixed-use development with ground floor commercial space in a C/R Commercial/Residential District with associated variations to dimensional and use standards. The subject property is currently owned by the Village of Morton Grove. Redevelopment of the subject property will be contingent on the execution of redevelopment and purchase agreements between the applicant and Village. The property at 6419 Chestnut Street will continue to be owned by the Loyal Order of Moose (No. 376) and operated as the Morton Grove Moose Family Center.

### Subject Property

The subject property at 8500-50 Lehigh Avenue is located on the west side of Lehigh Avenue at its intersection with Chestnut Street and is generally situated between Lincoln Avenue to the north and Elm Street to the south. The Morton House Condominiums abut the subject property to the north and the Forest Preserves of Cook County abut the property to the west and south. The Morton Grove Metra station and commuter parking lot are located directly across Lehigh Avenue from the subject property at 8501 Lehigh Avenue.

The overall development site measures approximately 1.9 acres and consists of the following three properties:

1. 8500 Lehigh Avenue (0.535 ac +/-): This Village-owned property is currently vacant and will be sold to the applicant for redevelopment.
2. 8550 Lehigh Avenue (0.983 ac +/-): This Village-owned property is currently improved with a commuter parking lot and will be sold to the applicant for redevelopment.
3. Chestnut Street Right of Way (0.531 ac +/-): This 66-foot Village right of way is an asphalted street in poor condition. The right of way is petitioned to be vacated under Case PC 25-08, with portions to be deeded to the future abutting property owners, the Loyal Order of Moose No. 376 and the applicant.



**Subject Property Location Map**

In March 2025, the Village issued a request for qualifications (RFQ) seeking a qualified developer to acquire and redevelop the Village-owned property at 8500-50 Lehigh Avenue for mixed-use, multi-family, or commercial development that aligns with the Village's vision for a pedestrian-friendly and transit-oriented downtown with a vertical and horizontal mix of uses. The applicant's submitted proposal was selected by staff as the preferred concept. Staff provided input on the initial concept and the applicant revised the project site plan and elevations to satisfy Village requirements and objectives.

Another developer was previously granted a special use permit for the subject property in November 2023 (Ord. 23-24). That special use permit was for a 36-unit mixed-use development, but no substantive progress was made in implementing the approved improvement and the permit expired.

### Project Overview

The applicant is proposing a four-story mixed-use development with 4,020 square feet of ground floor commercial space and 60 one- and two-bedroom residential units located throughout all four stories. The residential units are planned to be leased by the developer. The first-floor commercial uses are anticipated to include a mix of retail and restaurant space. An outdoor café/patio space is proposed for the north end of the building that would also provide additional fire lane access in case of emergency.

The developer is proposing 133 parking spaces to the rear of the principal structures, nine (9) of which will be located in the Chestnut Street right of way and are intended for use by the Moose Family Center property to offset spaces lost in reconfiguring access to the Moose site. The 124 spaces dedicated to the development exceed the reduced transit-oriented development (TOD) parking requirement of 98 to 106 parking spaces allowed by the site's proximity to the Metra station.

A Vacation Application submitted by the applicant, to be reviewed under Case PC 25-08, petitions the Village to vacate Chestnut Street in order to maximize the development site area. Access to the Moose Family Fun Center and mixed-use development is proposed to be provided by a new full-access driveway along the southern boundary of the site. The driveway will be privately owned and maintained by the developer. The proposed plaza at the north side of the building has been designed with mountable curb and a standard fire lane width to serve as an emergency accessway.

The applicant is requesting Special Use Permit for minor modifications to the general and C/R District standards for mixed-use development. At the time of application, the applicant had not executed any leases for the ground-floor commercial space. The future tenants will be subject to all requirements set forth in the Special Use Permit.

Site Design

Section 12-5-7:A.3 establishes fundamental design principals for development in the C/R District, which "is intended to encourage the creation of a vibrant mixed-use neighborhood that allows for convenient access to local businesses and the Metra station while giving priority to pedestrians and residents." The principals encourage a defined streetwall that creates a comfortable public space scaled for humans. The streetwall should be continuous, with gaps between buildings minimized, and the building design should be oriented to the pedestrian, with long stretches of blank and windowless walls to be avoided. Parking should be located behind buildings and site plans should be arranged to create focal points to guide pedestrians around corners and along the street. Developments should also provide a comfortable and safe sidewalk space with adequate room for streetscaping, public art, and outdoor seating.

The proposed site plan provides a setback of between 4.1 feet and 5.9 feet at the front lot line along much of Lehigh Avenue creating a strong streetwall. The development's surface parking will be located to the rear of the principal structures and out of view from Lehigh Avenue, so as not to disrupt the pedestrian nature of the streetwall. The use of a singular public vehicular accessway creates a continuity in the streetwall. Recessed balcony areas with small patios on the ground floor and related changes in material break up the façade. A large central building entrance, ground floor residential patios, and a high degree of transparency provided by large ground floor windows will also help provide for a sense of activity at the street level. Landscaping areas along the buildings' frontage and new street trees will also contribute to a vibrant and pedestrian-oriented public realm. The northern side setback will be heavily landscaped to provide buffering between the Morton House Condominiums, an abutting multi-family residential use to the north, and maintain visual interest along Lehigh Avenue.



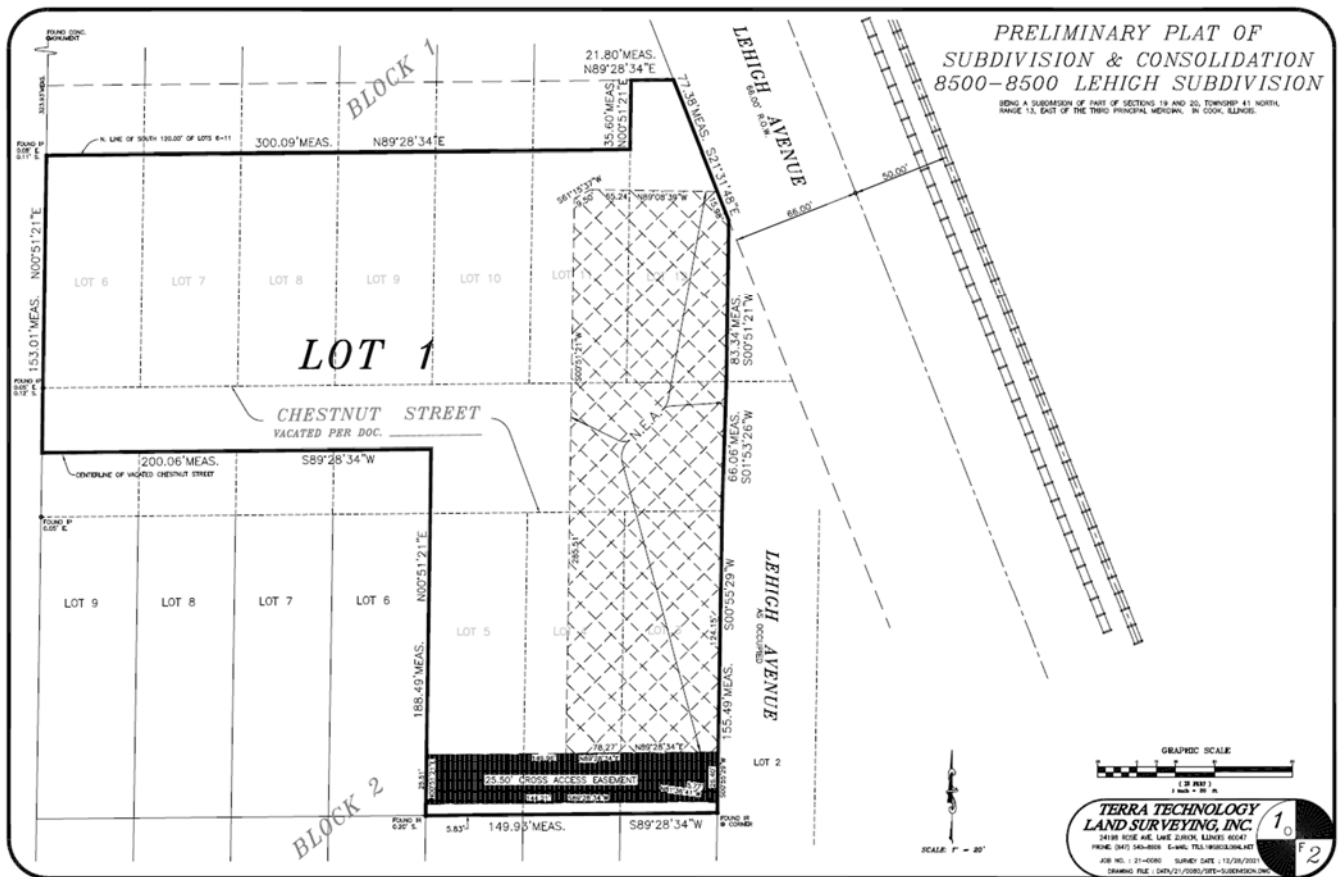
Proposed Site Plan

Preliminary & Final Plat of Subdivision

The applicant submitted a Preliminary Plat of Subdivision under PC 25-08 entitled "Preliminary Plat of Subdivision and Consolidation 8500-8550 Lehigh Subdivision," which proposes the consolidation of two (2) lots and the Chestnut Street right of way. The proposed subdivision and consolidation will allow the Loyal Order of Moose (No. 376) and the applicant to own those vacated portions of Chestnut Street to the centerline that abut their respective properties and consolidate all lots to be owned by the applicant, including 8500 Lehigh Avenue, 8550 Lehigh Avenue, and the larger portion of the vacated Chestnut Street right of way.

The Loyal Order of Moose (No. 376) has had preliminary meetings with the Village regarding the proposed easement and vacation of Chestnut Street. As currently proposed, the subdivision and consolidation will result in two lots, one of which will be deeded to the Loyal Order of Moose (No. 376) and the other to the applicant under forthcoming redevelopment and purchase agreements. A Final Plat of Subdivision will be submitted to the Village for review and approval following the completion of site improvements. The Final Plat may only be recorded following Board approval by ordinance.

*Approval of the Special Use Permit and Subdivision as part of PC 25-07 can move forward with a condition that approval is contingent upon the successful approval of the Plat of Vacation under PC 25-08.*



Proposed Preliminary Plat of Subdivision

Development Controls

The proposed project meets most dimensional requirements for mixed-use development in the C/R Commercial/Residential District. Applicable dimensional requirements and compliance are outlined in the following table. Staff notes that some dimensional discrepancies were found in the submitted documents. For dimensional purposes, staff deferred to the revised site plan submitted by BSB Design and dated August 7, 2025. Applicable requirements and the project's compliance are outlined in the following table:

C/R District Dimensional Controls	Requirement	Proposed	Compliance
Lot Area (12-5-7:C)	Max. 24 dwelling units per acre	31.6 du/ac	<b>Waiver needed for 7.6 du/ac</b>
Lot Width (12-5-7:C)	Min. 60 ft.	381.97 feet	<i>Compliant</i>
Front Setback (12-5-7:C)	Max. 10 ft.	5.6 ft.	<i>Compliant</i>
Interior Side Setback (12-5-7:C)	Min 5. feet for buildings up to 20 ft. in height; Add 1 foot setback for every 3 ft. in additional building height; Max. 10 ft.	North: 22.15 ft. South: 25 ft.	<b>North: Waiver of 7.85 ft. needed for setback of 22.15 ft.; South: Waiver of 5 ft. needed for setback of 25 ft.</b>
Building Height (12-5-7:C)	Max. 50 ft.	53.67 ft.	<b>Waiver of 3.67 ft. needed to permit a maximum height of 53.67 ft. in addition to rooftop-mounted mechanicals</b>
Floor Area Ratio (12-5-7:C)	N/A	0.81	<i>Compliant</i>
Impermeable Lot Coverage (12-5-7:C)	N/A	86.9%	<i>Compliant</i>
Detached Accessory Structures on a Zoning Lot (12-2-2:B)	Max. 2	3	<b>Waiver of 1 structure needed to permit 2 trash enclosures and 1 pergola</b>
Rear Yard Impermeable Coverage (12-2-5:B.3)	N/A	89%	<i>Compliant</i>
Open Accessory Parking Spaces (12-2-6:G)	Min. 3 ft. from all lot lines	2 ft.	<b>Waiver of 1 ft. needed to permit parking within 2 ft. of Moose Family Center Property</b>
Rooftop Appurtenances (12-2-8:A)	May exceed max. building height by max. 10 feet; must be screened	< 60 feet	<i>Compliant</i>

Outdoor Seating Areas

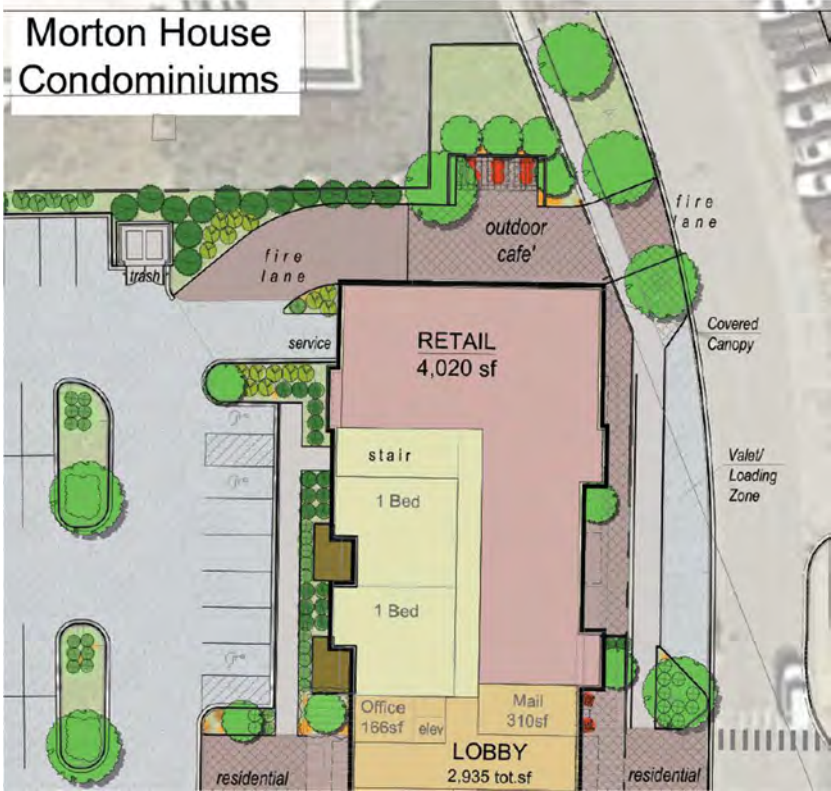
Per Section 12-5-5:C, if outdoor seating areas are in excess of 100 square feet, adequate landscaping and screening must be provided, subject to review and approval by the Appearance Commission. Outdoor seating area requirements are outlined in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Outdoor Seating Area Location (12-5-5:C.1)	Not permitted in public right of way	Not proposed in public right of way	<i>Compliant</i>
Outdoor Seating Area Design (12-5-5:C.2-4)	Not permitted in a required landscape area, must be on a hard surface, must provide min. 3 ft. pedestrian access	Outdoor seating not in a required landscape area, on a concrete sidewalk, min. 3 ft. pedestrian access	<i>Compliant</i>
Advertisement (12-5-5:C.13)	No advertising is permitted on umbrellas or screening	No advertising proposed	<i>Compliant</i>

It is not clear whether portions of the Lehigh Avenue frontage would be used for outdoor seating, or if that would be restricted to the seating area identified along the northern edge of the property. While public seating benches may be desirable within the streetscape area, any seating within the Lehigh Avenue right of way intended for dedicated use by business patrons or residents within the development would require a license agreement with the Village. The Village may impose additional requirements to mitigate liability for private use of Village-owned property and the final outdoor seating area plan will be subject to approval by the Village Administrator.

***The applicant should confirm the potential locations of future outdoor seating and speak to what kind of protections and screening will be provided for diners within the outdoor seating areas in the Lehigh Avenue public right of way.***

Staff recommends that as a condition of approval, all final outdoor seating area plans, including screening and furniture specifications, must be reviewed and approved by the Appearance Commission Chairperson. The Chairperson may require full Appearance Commission review if the quality and design of the outdoor seating areas is not in keeping with the quality and design of the overall development. Language regarding planter box maintenance is also included as a condition.

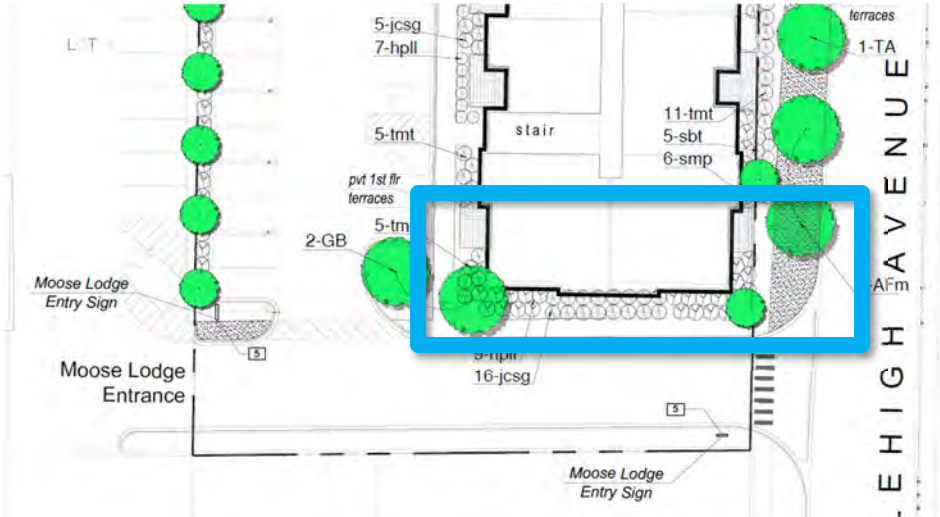


***Detail of Proposed Outdoor Seating Area***

**NOTE:**

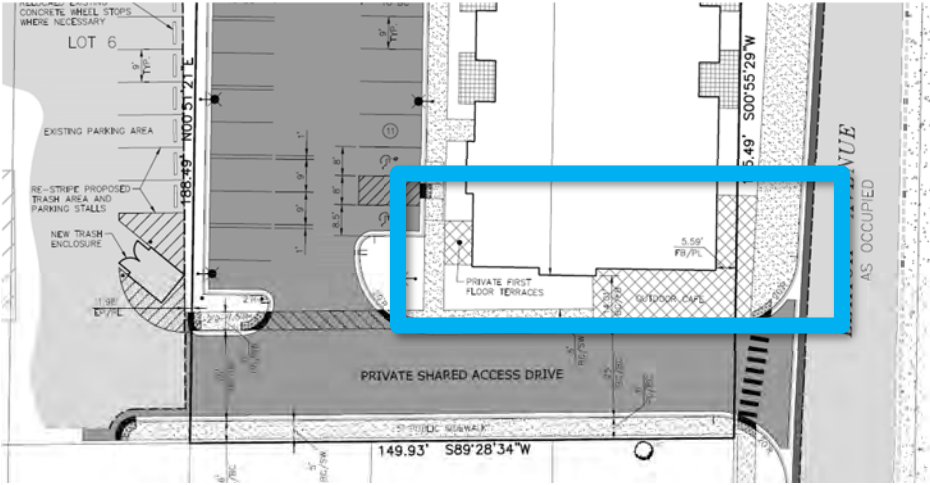
The site plan and landscape plan and related architectural drawings were revised by the applicant to eliminate a retail space and related outdoor seating area at the south end of the building. Staff has request revised civil drawings that replace the previous, outdated building extent and landscaping.

The sidewalk on the south side of the access drive along the south property line was also removed in the current application.



**Building Design**

In response to the RFQ and marketing of the site in previous years, the Village received proposals for much higher density multi-family and mixed-use development. Staff finds the four-story mixed-use building proposed by the applicant to be scaled and sited in a manner that achieves a "downtown" feel but is respectful to surrounding structures and uses. The development will not impede the views of the six-story Morton House Condominiums or overshadow the single-story Moose Family Center.



*Revised site plan (TOP); Outdated site plan in civil drawings (BOTTOM)*

Architectural details such as horizontal coping and banding, awnings, and balconies also help break up the building facades and create visual interest. The following materials are being proposed as indicated on the proposed elevations:

- Stucco and engineered panel siding – James Hardie
- Brick veneer – Belden "Brandywine Velour"
- Soldier course brick caps
- Aluminum storefront window system – Kawneer "Dark Bronze"
- Fabricated balconies – Midwest Iron
- Metal parapet cap

***The applicant is expected to provide additional details regarding the proposed materials and address the durability and long-term maintenance of the materials proposed.***

Exterior Materials Legend	
	Brick Veneer Belden Brandywine Velour
	Windows and Balcony Doors Anderson Fibrex or Equivalent
	Sconce Lighting Chara 12 Outdoor
	Aluminum Storefront Kawneer - Dark Bronze
	Fabricated Balconies & Metals Midwest Iron
	Stucco Panel Siding James Hardie
	Engineered Panel Siding James Hardie

**Bird-friendly Design**

To mitigate bird collisions with the buildings' window area, especially considering the development's proximity to natural areas, staff recommends as a *condition of approval that the development must adhere to bird-friendly design guidelines contained in the "Bird-Friendly Building Design" manual of the American Bird Conservancy* (2015, [https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide\\_2015.pdf](https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide_2015.pdf)) where practicable. In discussion with the Appearance Commission, the applicant indicated that dot-pattern glazing would be used to achieve a bird-friendly design.



*Proposed Development Rendering – Lehigh Avenue frontage (viewed from northeast)*



*Proposed East Elevation with Material Callouts (North end of Building)*

**Facade Transparency**

Section 12-5-7:A.3.k of the Morton Grove Municipal Code establishes minimum requirements for facade transparency for mixed-use developments in the C/R district. Facade transparency creates a visual connection between indoor and outdoor spaces, enhances a building's aesthetic appeal, and fosters a sense of security and vibrancy for pedestrians. The proposed elevations for the north and east façades are compliant, however the south and west façades are 3.2% and 3.3% below the required 50% transparency respectively. On the east elevation facing Lehigh Avenue, within the specified area between two and 12 ft. above grade, is 53.8% transparent. The applicant is requesting waivers to the minimum percentage of facade transparency required by Code to allow the building elevations as presented.

The applicant has indicated a desire to use obscured or frosted glass on portions of the first floor facing Lehigh Avenue including windows facing the fitness area. An overview of proposed facade transparency as it relates to Village Code requirements is provided in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Facade Transparency (12-5-7:A.3.k)	Min. 50% of wall area between 2 and 12 feet above grade shall be occupied by windows or entry doors	East elevation (street): 53.8% North elevation (plaza): 71.9% West elevation (parking): 46.7% South elevation (access drive): 46.8%	East elevation (street): Compliant North elevation (plaza): Compliant <b>West elevation (parking): 3.3%</b> <b>South elevation (access drive): 3.2%</b>
Clarity (12-5-7:A.3.k.1)	Clear transparent glass	Unknown	<b>Potential waiver to allow obscure glass for fitness area only</b>

Landscape Design

The applicant submitted a landscape plan prepared by RWG Engineering, LLC. The applicant is requesting two waivers to the landscaping requirements set forth in Chapter 12-11: (1) a waiver of one tree to the minimum requirement for trees planted in the abutting public right of way and (2) a waiver to permit limited landscaping along the westernmost and southernmost lot lines abutting property owned and operated by the Forest Preserve District of Cook County (FPDCC).

Nine new street trees are proposed along Lehigh Avenue. Staff is concerned that additional trees within the public right of way will conflict with valet /loading zone that will be desirable for potential future restaurant operations at the project, and is supportive of the waiver of one tree.

With respect to the proposed waiver to allow no landscaping or alternative screening along lot lines abutting forest preserve property, staff recommends reducing the minimum screening height to three feet, where five feet are typically required by Code, or as otherwise recommended by Forest Preserve District staff. Staff is concerned that headlights from vehicles in the parking and driveway areas will be disturbing to these biologically sensitive areas and should be shielded. The Appearance Commission supported a waiver to allow for the landscape bed along the west lot line to maintain the proposed narrow width. **However, the Appearance Commission recommended as a condition of approval that the developer install fencing or year-round opaque screening a minimum of three feet in height. The Appearance Commission also recommended that any landscape installation along lot lines abutting the forest preserves must be limited to native species.**

**Native Plantings**

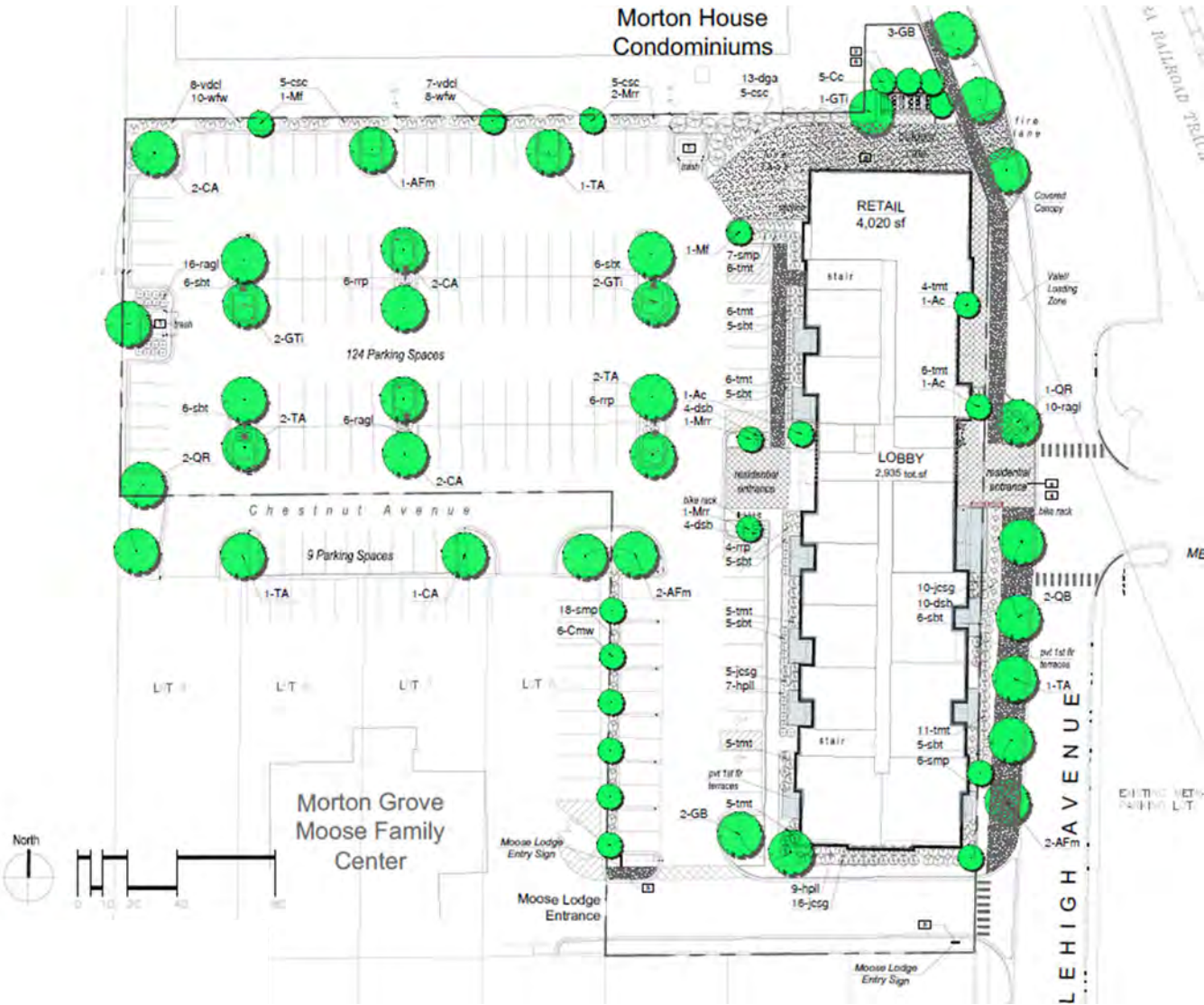
Due to the subject property's proximity to natural areas, staff recommends as a condition of Appearance Certificate approval that the submitted landscape plan include only species that are not invasive or undesirable. Staff reviewed the proposed plant list against the Northwest Illinois Forestry Association invasive plants list and did not identify any invasive species. Staff has concerns regarding the proposed use of "Grow-low Sumac." The same plant has been used in several projects in the Village and some larger installations such as in parking lot islands at the Sawmill Station retail center have experienced die-offs due to a fungus. **In discussion with the Appearance Commission, the applicant agreed to substitute another non-invasive groundcover species for the "Grow-low Sumac."**

The Village's applicable landscape requirements and requested waivers are outlined in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Landscape Area (12-11-2:B.1.a)	Min. 8% of total site in a TIF District, the majority of which is to be provided along the street ROW	8.1%	Compliant
Public Parkway Trees (12-11-1:B.4)	Parkway trees required with max. 40-ft. separation, min. 2.5-in. caliper	9 trees / 382.2 ft. frontage = avg. 42.5-foot separation	<b><i>Waiver of 1 tree and average 2.5 feet to maximum separation</i></b>
Irrigation (12-11-3:G)	Underground irrigation system or readily available water supply required	Hose bib access to maintain exterior	Compliant
Parking Lot Screening Abutting Private Property (12-11-4:B.2)	Landscaping or structure required, min. 5-ft. buffer yard with berm, hedge, maintenance free barrier 5-6 ft. in height	<u>North lot line:</u> 4 ft. landscape bed  <u>West lot line abutting FPDCC property:</u> 1.6 ft. setback, Min. 3 ft. screening  <u>West lot line abutting Moose property:</u> 5 ft. landscape bed  <u>South lot line abutting FPDCC property:</u> 6 ft. setback, no landscape buffer (groundcover only)	<b><i>North lot line: Waiver of 1 ft. to allow a landscape screen of 4 ft.; West lot line abutting FPDCC: Waiver of 3.4 ft. to allow a landscape screen of 1.6 ft. West lot line abutting Moose property: Compliant South lot line abutting FPDCC property: Waiver to permit groundcover only</i></b>
Parking Lot Landscaping Islands (12-11-4:B.3)	Min. 40 sq. ft.; 1 tree per single row island; 2 trees per double row island	189 – 500 sq. ft. 1-2 trees pe	Compliant
Parking Lot Landscaping Area (Lot > 20,000 SF) (12-11-4:B.3)	Min. 7% of paved area, not including buffer landscape area (or trash enclosures)	9.4.2%	Compliant
Parking Bay Length (12-11-4:B.3)	Max. 20 spaces in an uninterrupted row	Bays < 20 spaces	Compliant
Screening (12-11-4)	Min. 5-ft. screening for all loading docks, equipment enclosures, and trash areas (see 12-11-4 for allowed screening types)	Trash areas are interior or screened	Compliant

***Tree Preservation***

The applicant did not provide the required tree survey and tree preservation plan, but committed to meeting Village tree preservation requirements. Protected trees are defined as non-nuisance species with a diameter at breast height of 12 inches or greater per the Village's Tree Ordinance (Ord. 24-28). Staff site visits indicate that a total of two protected trees are on the subject property, located on the east end of the Village-owned parking lot at 8550 Lehigh. Those trees will need to be removed during construction as they are within the footprint of the proposed building. As such, tree replacements or a fee-in-lieu will be required for any lost protected trees. ***The Tree Protection and Tree Preservation Plans should be required as a condition of approval with compliance subject to review and approval by the Community Development Administrator.***



Proposed Landscape Plan

On-site Lighting

The development's proposed lighting plan should be discussed, especially due to the subject property's adjacency to biologically sensitive forest preserves owned and operated by the FPDC. The applicant submitted a photometric plan showing levels of illumination along lot lines abutting forest preserve property measuring up to 1.4 foot-candles. Levels along the north lot line abutting the Morton House Condominiums measure up to 1.3 foot-candles and levels along lot lines abutting Moose Family Center property measure up to 1.0 foot-candles.

Per Section 12-4-3:B.5, lighting of parking and loading areas must be a minimum of one foot-candle on the surface. However, such lighting must be confined to the property boundary and reach as close to zero illumination at the property boundaries as possible. Glare may not be evident from surrounding properties or adjacent public rights of way.

Section 12-12-3 of the Unified Development Code establishes lighting fixture standards. For off-street parking areas, lighting must be directed away from adjacent property, streets, and other public rights-of-way. All lighting units must be of the full cutoff type, meaning luminaires may not emit any light above the source's horizontal plane. The International Dark-Sky Association (IDA) recommends full cutoff fixtures, which minimize glare and light trespass. **The applicant did not provide specification sheets for the selected site lighting fixtures and should confirm whether the proposed site lighting fixtures qualify as full cutoff fixtures.**

**Staff recommend as a condition of approval that the applicant submit a revised lighting plan that includes required specification sheets to meet all lighting requirements of the Village of Morton Grove including achieving close to zero illumination at the property lines, subject to approval by the Village Engineer.** The lighting plan and fixtures should also comply with all standards established in IDA's lighting guidelines and any exterior lighting should be designed to minimize the amount of light entering into the forest preserves, to the best extent practicable.

### Traffic and Parking Impact

A traffic and parking impact study was provided by Kimley-Horn and Associates, Inc. For Special Uses, the off-street parking requirements set forth in Section 12-7-3:1 are advisory only and the final parking required for each use is decided by the Village Board based on the submitted study, any traffic and parking recommendation prepared by the Village staff, and the final recommendation of the Plan Commission.

### **Traffic**

Kimley-Horn concluded that the site traffic expected to be added to Lehigh Avenue is not anticipated to significantly impact operations along the study area roadway. The study notes that per the Highway Capacity Manual (HCM), Lehigh Avenue has a capacity of about 10,000 vehicles per day before experiencing significant congestion and delay. The study finds that the addition of the proposed mixed-use development would result in Lehigh Avenue carrying approximately 6,193 vehicles per day with additional capacity for nearly 3,800 trips. Based on the traffic projections, Lehigh Avenue is anticipated to accommodate the site traffic without material impacts to its operations.

The study recommends that the installation of pedestrian crosswalk striping along the south leg of the Lehigh Avenue and Chestnut Avenue intersection be considered as the residents of the proposed multi-family housing would use this intersection to access the adjacent Morton Grove Metra station. Further, it is recommended that the proposed private shared access drive serving the development should operate under minor-leg stop control operations with Lehigh Avenue and provide a stop sign, stop bar, and pedestrian crosswalk striping along the west leg.

### **Parking**

The developer is proposing 133 parking spaces to the rear of the principal structures, nine (9) of which will be located in the Chestnut Street right of way and are intended for use by the Moose Family Center property to offset spaces lost in reconfiguring access to the Moose site. The 124 spaces dedicated to the development is short of the Code's base parking requirement of 130 parking spaces if the commercial space were used for retail. If the commercial space were used entirely as restaurant space then the total base parking required would be 141 spaces. Snow storage removal areas are proposed on 13 of the 124 parking spaces, potentially reducing parking capacity to 111 spaces during periods of heavy snow accumulation. Regardless of the base parking requirement used, the development exceeds the reduced transit-oriented development (TOD) parking requirement of 98 to 106 parking spaces allowed by the site's proximity to the Metra station.

### **Moose Family Center Parking**

Based on preliminary feedback provided by Staff, the applicant revised the development proposal to include nine (9) spaces for the Moose Family Center. The proposed spaces would more than offset parking spaces lost to the new access drive and a proposed trash enclosure.

### **Snow Storage, Trash Removal, and Deliveries**

Snow storage will be located on-site within the 13 parking spaces at the northwest corner of the property. Snow removal on the development site, including the shared access drive, will be the ongoing responsibility of the applicant or future property owner. A trash enclosure is proposed for the northern edge of the parking lot adjacent the proposed fire lane. A second trash enclosure is proposed for the center portion of the western edge of the parking lot. **The applicant should speak to the proposed storage of snow removal equipment and salt and routing of service and delivery traffic.**

### Stormwater and Utilities

RWG Engineering, LLC, submitted a Preliminary Utility Plan, which illustrates proposed detention volume, volume control, and release rates for the stormwater improvements. The applicant has acknowledged that the development needs to be designed in accordance with Metropolitan Water Reclamation District (MWRD) and Village requirements. The underground detention is proposed to be located below the surface parking area.

The design of the sanitary sewer and water main will be coordinated with the Village. Based on the Village's communications in the request for qualifications for the subject property, RWG Engineering is proposing separated water and sewer lines to the development. While the initial design shows a single sanitary sewer line connecting both the new development and the Moose Family Center to Lehigh Avenue, however the Village Engineer has indicated a separate sanitary sewer connection to be maintained by the Moose Family Center would be preferred. Additional coordination is needed, but in general the utilities are adequate to serve the project.

As is required for new development, the Village is requiring that existing aboveground utilities be buried, subject to review and approval by the Village Engineer. The Village will be initiating a longer term infrastructure planning process for Lehigh Avenue and developer coordination for utility alignments to and public improvements along Lehigh Avenue should be a condition of approval.

### Commission Review

#### ***Appearance Commission***

On September 2, 2025, the Appearance Commission reviewed Case PC 25-07. At the conclusion of the discussion, the Appearance Commission recommended approval (5-0) of the application with recommended conditions. The Staff Report to the Appearance Commission has been included as "Attachment A."

#### ***Traffic Safety Commission***

On September 4, 2025, the Traffic Safety Commission (TSC) reviewed Case PC 25-07 and the Traffic Impact Study. At the conclusion of the discussion, the TSC voted unanimously (5-0) to recommend approval of the application with comments (see "Attachment B").

### Departmental Review

The proposed project was reviewed by several department representatives with the Fire Department and Department of Public Works providing comments (see "Attachment C").

- Building Department: No comments at this time.
- Fire Department: In review of the proposed project, the Fire Chief indicated that, "*The Fire Department would be concerned with tight clearances to the rear of the building for emergency access. With the amount of parking spaces this could further hinder our access if numerous vehicles were trying to exit the only pathway during an emergency.*" In response to Fire Department comments, the applicant has revised the design to feature a fire lane on the north side of the building.
- Public Works Department/Engineering: In review of the proposed project, the Village Engineer issued several comments dated June 30, 2025, regarding:
  - The need for updated turning diagram for a larger fire apparatus.
  - Additional analysis and discussion regarding proposed sewer alignments for the proposed development and the Moose Family Center.
  - Coordinating with the Village on right of way improvements along Lehigh Avenue.

## Standards for Review

The Standards for Subdivision are established in Section 12-16-4:D.3 of the Unified Development Code:

12-16-4:D.3. Standards for Subdivisions: The following standards for evaluating subdivisions shall be applied in a reasonable manner, taking into consideration the restrictions and/or limitations which exist for the site being considered for development:

1. Orderly Development: The proposed subdivision will encourage orderly and harmonious development within the Village.
2. Coordination of Streets: The streets within the proposed subdivision will coordinate with other existing and planned streets within the Village.
3. Coordination of Utilities: The utilities within the proposed subdivision will coordinate with existing and planned utilities, and create a uniform system of utilities within the Village.
4. Consistency with Comprehensive Plan: The proposed subdivision will be evaluated based on its consistency with the overall land use policies of the Village as may be expressed in the Village's comprehensive plan.
5. Section 12-16-4:C.5 of the Unified Development Code establishes Standards for Special Uses, which are intended to be used for evaluating Special Use Permit requests. The Standards are as follows:

12-16-4:C.5. Standards for Special Uses: The following standards for evaluating special uses shall be applied in a reasonable manner, taking into consideration the restrictions and/or limitations which exist for the site being considered for development:

1. Preservation of Health, Safety, Morals, And Welfare: The establishment, maintenance and operation of the special use will not be detrimental to or endanger the public health, safety, morals or general welfare.
2. Adjacent Properties: The special use should not be injurious to the use and enjoyment of other property in the immediate vicinity for the uses permitted in the zoning district.
3. Orderly Development: The establishment of the special use will not impede normal and orderly development or impede the utilization of surrounding property for uses permitted in the zoning district.
4. Adequate Facilities: Adequate utilities, access roads, drainage and other necessary facilities are in existence or are being provided.
5. Traffic Control: Adequate measures have been or will be taken to provide ingress and egress designed to minimize traffic congestion on the public streets. The proposed use of the subject site should not draw substantial amounts of traffic on local residential streets.
6. Adequate Buffering: Adequate fencing and/or screening shall be provided to ensure the right of enjoyment of surrounding properties to provide for the public safety or to screen parking areas and other visually incompatible uses.
7. Conformance to Other Regulations: The special use shall, in all other respects, conform to applicable provisions of this title or amendments thereto. Variation from provisions of this title as provided for in subsection 12-16-3A, "Variations", of this chapter, may be considered by the plan commission and the Village board of trustees as a part of the special use permit.

The applicant should be prepared to discuss how the project meets the above standards at the Plan Commission public hearing.

Recommendation

Should the Plan Commission recommend approval of this application, staff suggests the following motion and conditions:

*Motion to recommend approval of Case PC 25-07, a request for approval of a Preliminary Plat of Subdivision, in accordance with Chapter 12-8 of the Morton Grove Municipal Code, and Special Use Permits with associated waivers for a 60-unit mixed-use development with ground floor commercial space for the property commonly known as 8500-8550 Lehigh Avenue in Morton Grove, Illinois, subject to the following conditions:*

- 1. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final elevations and material specifications for review and approval. Final elevations and materials must be deemed consistent with the approved elevations and materials, as determined by the Community Development Administrator and Appearance Commission Chairperson. If such designs are deemed to be inconsistent with the approved plans or if materials are deemed to be of a lower quality than the approved materials, then the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.*
- 2. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with a final landscape plan, including required Tree Protection and Tree Preservation Plans, for review and approval by the Community Development Administrator and Appearance Commission Chairperson. If the landscape plan is deemed to be inconsistent with the approved plan or has not been modified to remove any invasive or undesirable species, the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.*
- 3. The applicant shall install either fencing or year-round opaque screening a minimum of three feet in height within the proposed perimeter landscape bed along the west property line adjacent the FPDCC property. Any landscape installation along lot lines abutting the forest preserves must be limited to native species.*
- 4. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final outdoor seating area plans for review and approval. Final seating arrangements, screening, and furniture specifications must be deemed consistent with the overall development, as determined by the Community Development Administrator and Appearance Commission Chairperson. If such designs are deemed to be inconsistent with the approved plans or if materials are deemed to be of a lower quality than the approved materials, then the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.*
- 5. If planter boxes or containers are provided, they must be fully planted with live vegetation when the outdoor seating area is in active use. When the outdoor seating area is not in active use, the planter boxes or containers must be (1) planted with an alternative seasonal decoration, (2) covered, or (3) removed.*
- 6. Only frosted glass or a similar type treatment shall be used for window areas permitted to be obscured including a ground floor fitness area facing Lehigh Avenue. The glass shall not be obscured with any mirrored coating, vinyl applique, artwork, or signage.*
- 7. Illuminated signage and other illuminating features on the property may not exceed 5,000K (degrees Kelvin).*
- 8. Any portable signage shall be permitted pursuant to Section 10-10-8:E, except that the signage frame and base shall be constructed primarily of metal or wood, or as otherwise authorized by the Appearance Commission Chairperson.*
- 9. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final sign plan indicating the location of monument signs that adhere to all setbacks and landscaping requirements. Final sign plans must be deemed consistent with Appearance Commission discussion, as determined by the Community Development Administrator. Sign colors shall blend with the building and storefront colors through use of complementary color ranges, or as otherwise approved by the Appearance Commission Chairperson. If the sign plan is deemed to be inconsistent with the approved plans, then the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.*
- 10. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final lighting plan and photometric analysis that meets the minimum requirements of Village Code for review and approval by the*

*Community Development Administrator and Village Engineer. The lighting plan and fixtures should also comply with all standards established in IDA's lighting guidelines and any exterior lighting should be designed to minimize the amount of light entering into the forest preserves, to the best extent practicable.*

11. *To mitigate bird collisions with the buildings' window area, the development must adhere to bird-friendly design guidelines contained in the "Bird-Friendly Building Design" manual of the American Bird Conservancy (2015, [https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide\\_2015.pdf](https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide_2015.pdf)) where practicable. Mirrored coatings may not be used, but inconspicuous window films featuring simple dot or lined patterns are strongly encouraged.*
12. *Prior to filing any Building Permit Application, the owner/applicant shall submit revised site and utility plans that indicate existing aboveground utilities will be relocated underground as required by the Village, subject to review and approval by the Village Engineer.*
13. *Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with a revised site and utility plans that indicate the proposed location of street lighting along Lehigh Avenue frontage, or engage in an agreement with the Village to reimburse the Village for the installation of street lighting, subject to review and approval by the Village Engineer.*
14. *Conspicuous design elements shall be included in the final site design to provide awareness of the need to maintain the 24-foot-wide fire lane within the plaza clear of temporary or permanent fixtures that could interfere with emergency access, subject to review and approval by the Village Administrator.*
15. *The applicant shall comply with all comments issued by the Village Engineer in the communication dated June 20, 2025, by strict or alternative compliance, subject to the Village Engineer's final approval.*
16. *Approval shall be contingent on Board of Trustees approval of a vacation of a portion of Chestnut Street right of way located directly west of Lehigh Avenue right of way measuring approximately 0.531 acres, as presented under Case PC25-08.*
17. *[Any other condition(s) deemed appropriate by the Plan Commission]*

#### Attachments

- Attachment A – Staff Report to the Appearance Commission for PC 25-07, prepared by Brandon Nolin, AICP, Community Development Administrator, dated August 26, 2025
- Attachment B – Plan Review Comment Form for PC 25-07, prepared by Amit Shah, Traffic Safety Commission Chair dated September 5, 2025
- Attachment C – Plan Review Comment Forms for PC 25-07, prepared Department Heads
- Attachment D – Final Plans and Supporting Documents for PC 25-07

Attachment A  
Staff Report to the Appearance Commission for PC 25-07  
Prepared by Brandon Nolin, AICP, Community Development Administrator  
Dated August 26, 2025

To: Chairperson Pietron and Members of the Appearance Commission

From: Brandon Nolin, AICP, Community Development Administrator  
Anne Ryder Kirchner, Planner/Zoning Administrator

Date: August 27, 2025

Re: Appearance Commission Case AC 25-08

Request for an Appearance Certificate for site, building, landscape plans with associated waivers for a 60-unit mixed-use development proposed under Applications for Special Use and Subdivision (PC 25-07) for the property commonly known as 8500-50 Lehigh Avenue (PIN 10-19-204-020-0000, 10-19-203-021-0000, 10-19-203-022-0000, 10-19-203-023-0000, 10-19-203-025-0000, 10-19-203-013-0000) and a portion of Chestnut Street right of way petitioned for vacation (PC 25-08) in Morton Grove, Illinois. The applicant is 8500 MG, LLC.

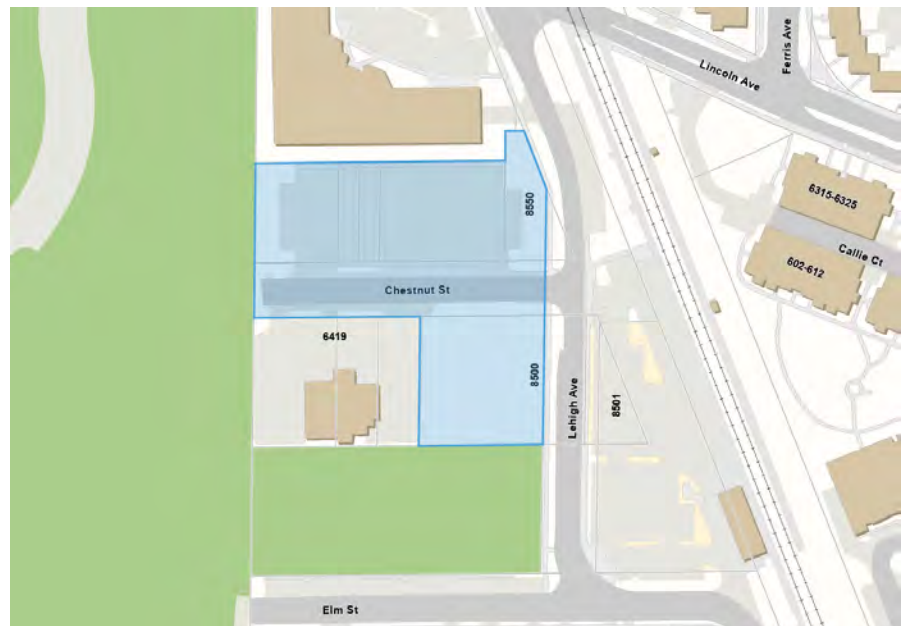
## STAFF REPORT

### Application Summary

8500 MG, LLC (“applicant”) submitted complete Vacation, Subdivision, and Special Use Applications to the Department of Community and Economic Development requesting approval of a vacation of a portion of Chestnut Street right of way, a subdivision and consolidation of vacated and Village-owned property, and Special Use Permits to allow the construction of a 57-unit mixed-use development with ground floor commercial space in a C/R Commercial/Residential District with associated variations to dimensional and use standards. The subject property is currently owned by the Village of Morton Grove. Redevelopment of the subject property will be contingent on the execution of redevelopment and purchase agreements between the applicant and Village. The property at 6419 Chestnut Street will continue to be owned by the Loyal Order of Moose (No. 376) and operated as the Morton Grove Moose Family Center.

### Subject Property

The subject property at 8500-50 Lehigh Avenue is located on the west side of Lehigh Avenue at its intersection with Chestnut Street and is generally situated between Lincoln Avenue to the north and Elm Street to the south. The Morton House Condominiums abut the subject property to the north and the Forest Preserves of Cook County abut the property to the west and south. The Morton Grove Metra station and commuter parking lot are located directly across Lehigh Avenue from the subject property at 8501 Lehigh Avenue.



Subject Property Location Map

The overall development site measures approximately 2.05 acres and consists of the following three properties:

1. 8500 Lehigh Avenue (0.535 ac +/-): This Village-owned property is currently vacant and will be sold to the applicant for redevelopment.
2. 8550 Lehigh Avenue (0.983 ac +/-): This Village-owned property is currently improved with a commuter parking lot and will be sold to the applicant for redevelopment.
3. Chestnut Street Right of Way (0.531 ac +/-): This 66-foot Village right of way is an asphalted street in poor condition. The right of way is petitioned to be vacated under Case PC 25-08, with portions to be deeded to the future abutting property owners, the Loyal Order of Moose No. 376 and the applicant.

In March 2025, the Village issued a request for qualifications (RFQ) seeking a qualified developer to acquire and redevelop the Village-owned property at 8500-50 Lehigh Avenue for mixed-use, multi-family, or commercial development that aligns with **the Village's vision for a pedestrian-friendly and transit-oriented downtown with a vertical and horizontal mix of uses.** The **applicant's submitted proposal was selected by staff as the preferred concept.** Staff provided input on the initial concept and the applicant revised the project site plan and elevations to satisfy Village requirements and objectives.

Another developer was previously granted a special use permit for the subject property in November 2023 (Ord. 23-24). That special use permit was for a 36-unit mixed-use development, but no substantive progress was made in implementing the approved improvement and the permit expired.

### Project Overview

The applicant is proposing a four-story mixed-use development with 4,020 square feet of ground-floor commercial space and 60 one- and two-bedroom residential units located throughout all four stories. The residential units are planned to be leased by the developer. The first-floor commercial uses are anticipated to include a mix of retail and restaurant space. An outdoor café/patio space is proposed for the north end of the building that would also provided additional fire lane access in case of emergency.

The developer is proposing 133 parking spaces to the rear of the principal structures, nine (9) of which will be located in the Chestnut Street right of way. **The development exceeds the Code's base parking requirement of 130 parking spaces** if the commercial space were used for retail. If the commercial space were used entirely as restaurant space then the total base parking required would be 141 spaces. Regardless of the base parking requirement used, the development exceeds the reduced transit-oriented development (TOD) parking requirement of 98 to 106 **parking spaces allowed by the site's proximity to the Metra station.**

A Vacation Application submitted by the applicant, to be reviewed under Case PC 25-08, petitions the Village to vacate Chestnut Street in order to maximize the development site area. Access to the Moose Lodge and mixed-use development is proposed to be provided by a new full-access driveway along the southern boundary of the site. The driveway will be privately owned and maintained by the developer. The proposed plaza at the north side of the building has been designed with mountable curb and a standard fire lane width to serve as an emergency accessway.

The development meets Village requirements for density, lot width, and setbacks. The applicant is requesting Special Use Permit for minor modifications to the general and C/R District standards for mixed-use development. At the time of application, the applicant had not executed any leases for the ground-floor commercial space. The future tenants will be subject to all requirements set forth in the Special Use Permit.

### Site Design

Section 12-5-7:**A.3 establishes fundamental design principals for development in the C/R District, which "is intended to encourage the creation of a vibrant mixed-use neighborhood that allows for convenient access to local businesses and the Metra station while giving priority to pedestrians and residents."** **The principals encourage a defined streetwall that creates a comfortable public space scaled for humans.** The streetwall should be continuous, with gaps between buildings minimized, and the building design should be oriented to the pedestrian, with long stretches of blank and windowless walls to be avoided. Parking should be located behind buildings and site plans should be arranged to create focal points to guide pedestrians around corners and along the street. Developments should also provide a comfortable and safe sidewalk space with adequate room for streetscaping, public art, and outdoor seating.

The proposed site plan provides a setback of between 4.1 feet and 5.9 feet at the front lot line along much of Lehigh Avenue creating a strong streetwall. **The development's surface parking will be located to the rear of the principal structures and out of view from Lehigh Avenue**, so as not to disrupt the pedestrian nature of the streetwall. The use of a singular public vehicular accessway creates a continuity in the streetwall. Recessed balcony areas with small patios on the ground floor and related changes in material break up the façade. A large central building entrance, ground floor residential patios, and a high degree of transparency provided by large ground floor windows will also help provide for a sense of activity at the street level. **Landscaping areas along the buildings' frontage and new street trees will also contribute to a vibrant and pedestrian-oriented public realm.** The northern side setback will be heavily landscaped to provide buffering between the Morton House Condominiums, an abutting multi-family residential use to the north, and maintain visual interest along Lehigh Avenue.

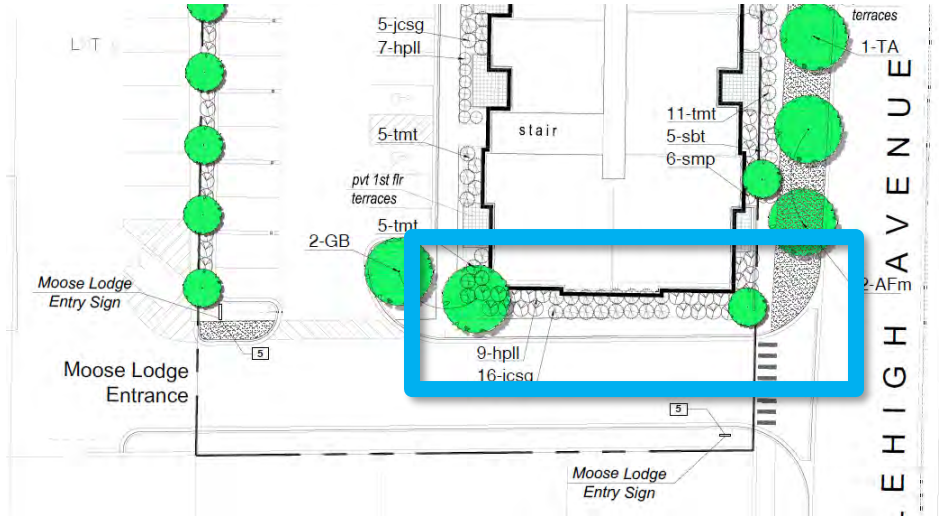


Proposed Site Plan

**NOTE:**

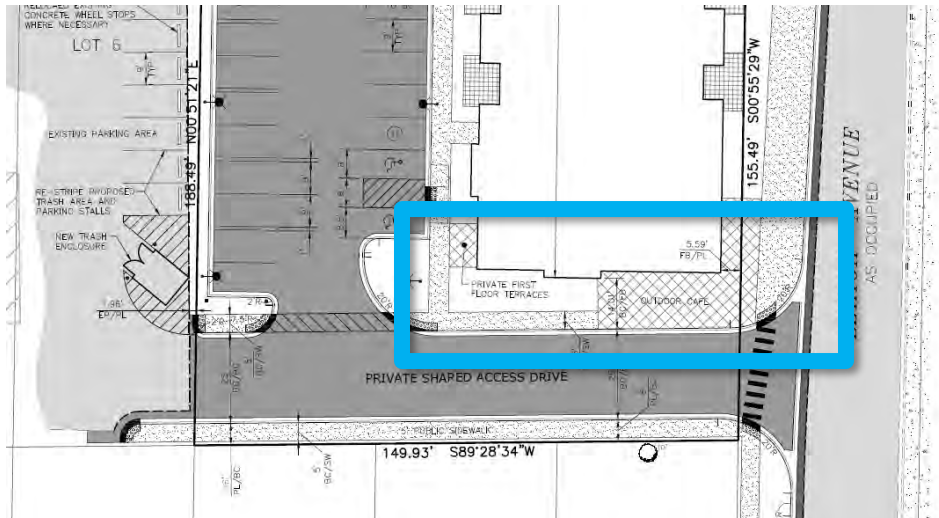
The site plan and landscape plan and related architectural drawings were revised by the applicant to eliminate a retail space and related outdoor seating area at the south end of the building. Staff has request revised civil drawings that replace the previous, outdated building extent and landscaping.

The sidewalk on the south side of the access drive along the south property line was also removed in the current application.



**Building Design**

In response to the RFQ and marketing of the site in previous years, the Village received proposals for much higher density multi-family and mixed-use development. Staff finds the four-story mixed-use building proposed by the applicant to be scaled and sited in a manner that achieves a **“downtown” feel but is respectful to** surrounding structures and uses. The development will not impede the views of the six-story Morton House Condominiums or overshadow the single-story Moose Family Center.



Revised site plan (TOP); Outdated site plan in civil drawings (BOTTOM)

Architectural details such as horizontal coping and banding, awnings, and balconies also help break up the building facades and create visual interest. The following materials are being proposed as indicated on the proposed elevations:

- Stucco and engineered panel siding – James Hardie
- Brick veneer – **Belden “Brandywine Velour”**
- Soldier course brick caps
- Aluminum storefront window system – **Kawneer “Dark Bronze”**
- Fabricated balconies – Midwest Iron
- Metal parapet cap

*The applicant is expected to provide additional details regarding the proposed materials and address the durability and long-term maintenance of the materials proposed.*

**Exterior Materials Legend**

	Brick Veneer Belden Brandywine Velour
	Windows and Balcony Doors Anderson Fibrex or Equivalent
	Sconce Lighting Chara 12 Outdoor
	Aluminum Storefront Kawneer - Dark Bronze
	Fabricated Balconies & Metals Midwest Iron
	Stucco Panel Siding James Hardie
	Engineered Panel Siding James Hardie



*Proposed Development Rendering – Lehigh Avenue frontage (viewed from northeast)*



*Proposed East Elevation with Material Callouts (North end of Building)*

*Facade Transparency*

Section 12-5-7:A.3.k of the Morton Grove Municipal Code establishes minimum requirements for facade transparency for mixed-use developments in the C/R district. Facade transparency creates a visual connection between indoor and outdoor spaces, enhances a building's aesthetic appeal, and fosters a sense of security and vibrancy for pedestrians. Typically, facade transparency minimums are applied along elevations fronting on streets. In this case, the buildings were requested by staff to be designed in a manner where no elevation would be treated as a rear elevation. Window area was more equitably spread across all four elevations.

Per code, at least 50% of the wall area that is between two (2) and 12 feet above grade shall be occupied by windows and/or entry doors. The proposed elevations for the north and east façades compliant, however the south and west façades are 3.2% and 3.3% below the required 50% transparency. On the east elevation facing Lehigh Avenue, within the specified area between two and 12 ft. above grade, is 53.8% transparent. The applicant is requesting waivers to the minimum percentage of facade transparency required by Code to allow the building elevations as presented.

A fitness area is proposed for the first floor facing Lehigh Avenue. *The applicant should speak to any potential waivers needed to allow obscured or frosted glass for the fitness area or any other potential deviations from clear transparent glass.*

An overview of proposed facade transparency as it relates to Village Code requirements is provided in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Facade Transparency (12-5-7:A.3.k)	Min. 50% of wall area between 2 and 12 feet above grade shall be occupied by windows or entry doors	East elevation (street): 53.8% North elevation (plaza): 71.9% West elevation (parking): 46.7% South elevation (access drive): 46.8%	East elevation (street): Compliant North elevation (plaza): Compliant West elevation (parking): 3.3% South elevation (access drive): 3.2%
Clarity (12-5-7:A.3.k.1)	Clear transparent glass	Unknown	<i>Potential waiver to allow obscure glass for fitness and service spaces only</i>
Tinting & Screening (12-5-7:A.3.k.2)	Tint, internal screening, patterns, and mirrored coating prohibited	No tint, internal screening, patterns, or mirrored coating proposed	Compliant
Coatings (12-5-7:A.3.k.3)	Limited to minimum U-factor requirement in the State-adopted International Energy Conservation Code	Coating limited to minimum U-factor requirement	Compliant
Grade (12-5-7:A.3.k.4)	Commercial grade and design	Commercial grade and design	Compliant
Obstruction (12-5-7:A.3.k.5)	No obstruction beyond Chapter 10-10 permissions	No obstruction beyond Chapter 10-10 permissions	Compliant

*Bird-friendly Design*

To mitigate bird collisions with the buildings' window area, especially considering the development's proximity to natural areas, staff recommends as a condition of approval that the development must adhere to bird-friendly design guidelines contained in the **"Bird-Friendly Building Design" manual of the American Bird Conservancy** (2015, [https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide\\_2015.pdf](https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide_2015.pdf)) where practicable. Mirrored coatings may not be used and inconspicuous window films featuring simple dot or lined patterns are strongly encouraged.

## Landscape Design

The applicant submitted a landscape plan prepared by RWG, LLC. The applicant is requesting two waivers to the landscaping requirements set forth in Chapter 12-11: (1) a waiver of one tree to the minimum requirement for trees planted in the abutting public right of way and (2) a waiver to permit limited landscaping along the westernmost and southernmost lot lines abutting property owned and operated by the Forest Preserve District of Cook County (FPDCC).

Nine new street trees are proposed along Lehigh Avenue. Staff is concerned that additional trees within the public right of way will conflict with valet /loading zone that will be desirable for potential future restaurant operations at the project, and is supportive of the waiver of one tree. With respect to the proposed waiver to allow no landscaping or alternative screening along lot lines abutting forest preserve property, staff recommends reducing the minimum screening height to three feet, where five feet are typically required by Code, or as otherwise recommended by Forest Preserve District staff. Staff is concerned that headlights from vehicles in the parking and driveway areas will be disturbing to these biologically sensitive areas and should be shielded. *With the condition of approval recommended by Staff, the developer may install fencing or year-round opaque screening a minimum of three feet in height. Staff also recommends that any landscape installation along lot lines abutting the forest preserves must be limited to native species.*

The Village's applicable landscape requirements and requested waivers are outlined in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Landscape Area (12-11-2:B.1.a)	Min. 8% of total site in a TIF District, the majority of which is to be provided along the street ROW	8.1%	Compliant
Public Parkway Trees (12-11-1:B.4)	Parkway trees required with max. 40-ft. separation, min. 2.5-in. caliper	9 trees / 382.2 ft. frontage = avg. 42.5-foot separation	<i>Waiver of 1 tree and average 2.5 feet to maximum separation</i>
Irrigation (12-11-3:G)	Underground irrigation system or readily available water supply required	Hose bib access to maintain exterior	Compliant
Parking Lot Screening Abutting Private Property (12-11-4:B.2)	Landscaping or structure required, min. 5-ft. buffer yard with berm, hedge, maintenance free barrier 5-6 ft. in height	<u>North lot line:</u> 4 ft. landscape bed <u>West lot line abutting FPDCC property:</u> 1.6 ft. setback, no landscape buffer (groundcover only) <u>West lot line abutting Moose property:</u> 5 ft. landscape bed <u>South lot line abutting FPDCC property:</u> 6 ft. setback, no landscape buffer (groundcover only)	<i><u>North lot line:</u> Waiver of 1 ft. to allow a landscape screen of 4 ft.;</i> <i><u>West lot line abutting FPDCC:</u> Waiver of 3.4 ft. to allow a landscape screen of 1.6 ft.</i> <i><u>West lot line abutting Moose property:</u> Compliant</i> <i><u>South lot line abutting FPDCC property:</u> Waiver to permit groundcover only</i>
Parking Lot Landscaping Islands (12-11-4:B.3)	Min. 40 sq. ft.; 1 tree per single row island; 2 trees per double row island	189 – 500 sq. ft. 1-2 trees pe	Compliant
Parking Lot Landscaping Area (Lot > 20,000 SF) (12-11-4:B.3)	Min. 7% of paved area, not including buffer landscape area (or trash enclosures)	9.4.2%	Compliant
Parking Bay Length (12-11-4:B.3)	Max. 20 spaces in an uninterrupted row	Bays < 20 spaces	Compliant
Screening (12-11-4)	Min. 5-ft. screening for all loading docks, equipment enclosures, and trash areas (see 12-11-4 for allowed screening types)	Trash areas are interior or screened	Compliant



Lighting

The development's proposed lighting plan should be discussed, especially due to the subject property's adjacency to biologically sensitive forest preserves owned and operated by the FPDCC. The applicant submitted a photometric plan showing levels of illumination along lot lines abutting forest preserve property measuring up to 1.4 foot-candles. Levels along the north lot line abutting the Morton House Condominiums measure up to 1.3 foot-candles and levels along lot lines abutting Moose Family Center property measure up to 1.0 foot-candles.

Per Section 12-4-3:B.5, lighting of parking and loading areas must be a minimum of one foot-candle on the surface. However, such lighting must be confined to the property boundary and reach as close to zero illumination at the property boundaries as possible. Glare may not be evident from surrounding properties or adjacent public rights of way.

Section 12-12-3 of the Unified Development Code establishes lighting fixture standards. For off-street parking areas, lighting must be directed away from adjacent property, streets, and other public rights-of-way. All lighting units must be of the full cutoff type, meaning luminaires may not emit any light above the source's horizontal plane. The International Dark-Sky Association (IDA) recommends full cutoff fixtures, which minimize glare and light trespass. *The applicant did not provide specification sheets for the selected site lighting fixtures and should confirm whether the proposed site lighting fixtures qualify as full cutoff fixtures.*

The proposed sconce for building up lighting is shown to the right. Sconces are proposed to be placed between the majority of the windows and entrances on the east elevation facing Lehigh Avenue, as well as the west elevation facing the parking lot.

*Staff recommend as a condition of approval that the applicant submit a revised lighting plan that includes required specification sheets to meet all lighting requirements of the Village of Morton Grove including achieving close to zero illumination at the property lines, subject to approval by the Village Engineer. The lighting plan and fixtures should also comply with all standards established in IDA's lighting guidelines and any exterior lighting should be designed to minimize the amount of light entering into the forest preserves, to the best extent practicable.*



Outdoor Seating Areas

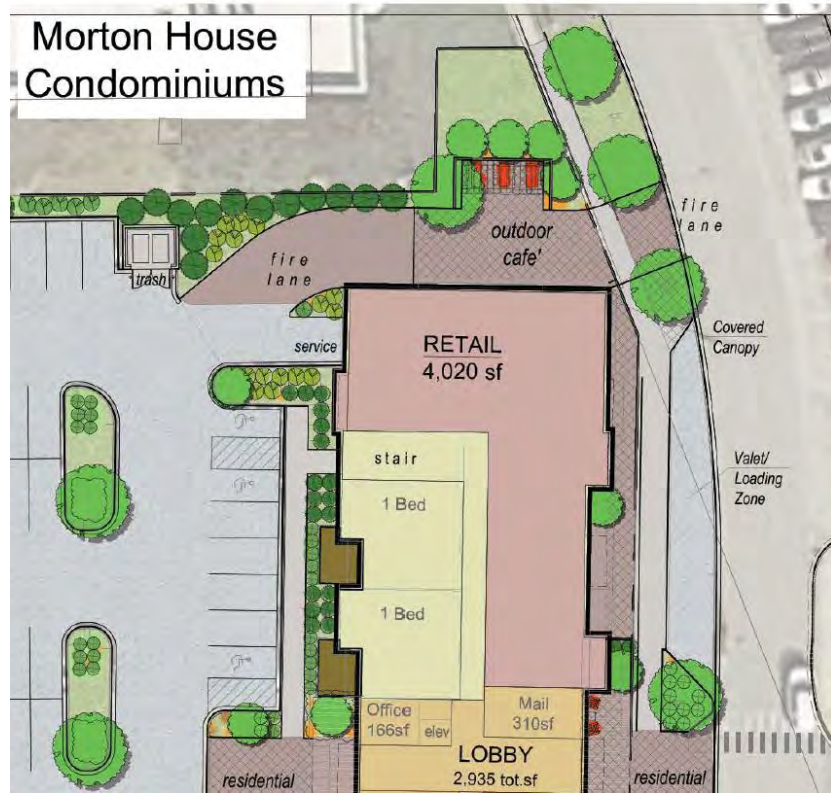
Per Section 12-5-5:C, if outdoor seating areas are in excess of 100 square feet, adequate landscaping and screening must be provided, subject to review and approval by the Appearance Commission. Outdoor seating area requirements are outlined in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Outdoor Seating Area Location (12-5-5:C.1)	Not permitted in public right of way	Not proposed in public right of way	Compliant
Outdoor Seating Area Design (12-5-5:C.2-4)	Not permitted in a required landscape area, must be on a hard surface, must provide min. 3 ft. pedestrian access	Outdoor seating not in a required landscape area, on a concrete sidewalk, min. 3 ft. pedestrian access	Compliant
Advertisement (12-5-5:C.13)	No advertising is permitted on umbrellas or screening	No advertising proposed	Compliant

It is not clear whether portions of the Lehigh Avenue frontage would be used for outdoor seating, or if that would be restricted to the seating area identified along the northern edge of the property. The applicant would be required to enter into a license agreement with the Village any use of the Lehigh Avenue public right of way for outdoor seating. The Village may impose additional requirements to mitigate liability for private use of Village-owned property and the final outdoor seating area plan will be subject to approval by the Village Administrator.

The applicant should confirm the potential locations of future outdoor seating and speak to what kind of protections and screening will be provided for diners within the outdoor seating areas in the Lehigh Avenue public right of way.

Staff recommends that as a condition of Appearance Certificate approval, all final outdoor seating area plans, including screening and furniture specifications, must be reviewed and approved by the Appearance Commission Chairperson. The Chairperson may require full Appearance Commission review if the quality and design of the outdoor seating areas is not in keeping with the quality and design of the overall development. Language regarding planter box maintenance is also included as a condition.



Detail of Proposed Outdoor Seating Area

### Signage

Because no specific tenants are being proposed at this time, a signage plan for the development was not submitted to the Appearance Commission for review. All signage is expected to comply with all requirements of Chapter 10-10 unless waivers are requested and approved by the Appearance Commission at a later date. The applicant should speak to the types of tenant and directional signage they anticipate and how the signage will be designed to be cohesive and complementary to the overall development.

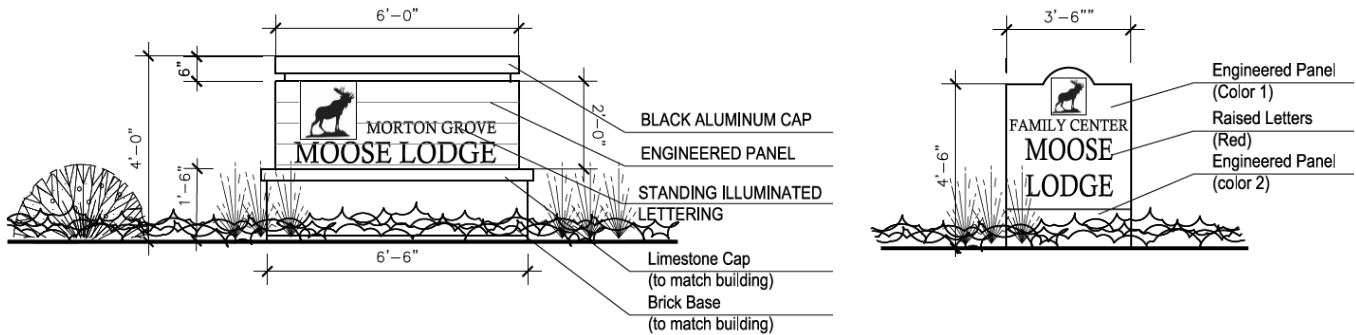
In order to maintain a high quality “downtown” feel, staff recommends conditions of approval that will enhance the overall quality of signage and attention-seeking ornamentation serving the development:

- 1) Sign colors shall blend with the building and storefront colors through use of complementary color ranges, or as otherwise approved by the Appearance Commission Chairperson.
- 2) Any portable signage shall be permitted pursuant to Section 10-10-8:E, except that the signage frame and base shall be constructed primarily of metal or wood, or as otherwise authorized by the Village Administrator.
- 3) Illuminated signage and other illuminating features on the property may not exceed 5,000K (degrees Kelvin).

To maintain visibility for the Moose Family Center from Lehigh Avenue, staff recommends the approval of various waivers to allow the installation of two signs by the developer without further review by the Appearance Commission: (1) the installation of a new entry monument sign just south of the new access drive to replace the existing off-premises pole sign on the east side of Lehigh Avenue, and (2) the installation of a new monument sign at the southwest corner of subject property, just north of the new access drive at the entrance to the Moose parking lot.

The above waivers are intended to authorize a replacement of the previous off-premises Moose Family Center pole sign with a monument sign that is compliant regarding dimensions, but that is located off-premises on property to be owned by the applicant. Should the Moose Family Center or applicant propose signage that is larger than described, additional waivers by the Appearance Commission may be required.

The proposed sign details indicate landscaping of some kind will surround the proposed monument sign. Code requires a minimum of a two-foot (2') landscape bed to surround each monument sign and be planted with shrubs at least three feet (3') in height at planting and may also include perennials, turf or other live ground cover. Staff is not supportive of a waiver for this requirement and has included this as a condition of approval.



Proposed Sign at entry to Moose Parking lot (Bottom left); Proposed sign at Lehigh Avenue (Bottom Right)

The Code requirements and requested waivers for the proposed off-premises monument signage are outlined in the following table.

Development Control	Requirement	Proposed	Waivers Requested
Monument Sign Quantity (10-10-7:G.2)	Max. 1 ground monument sign per 150 ft. of street frontage	2 monument signs per 382.2 ft. of street frontage	Compliant
Monument Sign Height (10-10-7:G.2)	Max. 10 ft. per sign combined, max. 25 ft. per sign individually	Access Drive: 4ft. Parking Lot Entrance: 4 ft.	Compliant
Monument Sign Area (10-10-7:G.2)	Max. 50 sq. ft. per sign face	Access Drive: 14 sq. ft. Parking Lot Entrance: 12 sq. ft.	Compliant
Monument Sign Structural Base (10-10-7:G.3)	Min. 75% base must be materials compatible with the building	Access Drive: 100% Parking Lot Entrance: 100%	Compliant
Monument Sign Location (10-10-7:G.6)	Min. 50% height or 4 ft. from public right of way, whichever is greater	Access Drive: 5 ft. from Lehigh Avenue Parking Lot Entrance: 5 ft. from Access Drive	Compliant
Monument Sign Landscape bed (10-10-7:G.5)	Min. 2 ft. from base, shrubs min. 3 ft. in height	Min 2 ft. from base on 3 sides of monument sign	<i>Waiver to allow no landscape bed</i>
Off-Premises Signs (10-10-5:B)	Off-premises signs prohibited	Proposed off-premises signage on south side of new access drive near Lehigh Avenue	<i>Waiver to allow replacement of Moose Family Center off-premises signage on adjacent private property</i>

## Appearance Commission Review

In accordance with Unified Development Code Section 12-12-1:C, all site, landscape and building plans are to be reviewed by the Appearance Commission, and an Appearance Certificate by the Commission granted, prior to the issuance of a building permit. Further, per Section 12-16-2:C.2, the Appearance Commission is charged with reviewing the exterior elevations, sketches, and materials and other exhibits as to whether they are appropriate to or compatible with the character of the immediate neighborhood and whether the submitted plans comply with the provisions of the regulations and standards set forth in chapter, 12 "Design Standards," of this title.

### The Design Standards (Sec. 12-12-1:D) are as follows:

D. Criteria and Evaluation Elements: The following factors and characteristics relating to a unit or development and which affect appearance, will govern the appearance review commission's evaluation of a design submission:

1. Evaluation Standards:
  - a. Property Values: Where a substantial likelihood exists that a building will depreciate property values of adjacent properties or throughout the community, construction of that building should be barred.
  - b. Inappropriateness: A building that is obviously incongruous with its surroundings or unsightly and grotesque can be inappropriate in light of the comprehensive plan goal of preserving the character of the municipality.
  - c. Similarity/Dissimilarity: A builder should avoid excessively similar or excessively dissimilar adjacent buildings.
  - d. Safety: A building whose design or color might, because of the building's location, be distracting to vehicular traffic may be deemed a safety hazard.
2. Design Criteria:
  - a. Standards: Appearance standards as set forth in this chapter.
  - b. Logic Of Design: Generally accepted principles, parameters and criteria of validity in the solution of design problems.
  - c. Architectural Character: The composite or aggregate of the components of structure, form, materials and functions of a building or group of buildings and other architectural and site composing elements.
  - d. Attractiveness: The relationship of compositional qualities of commonly accepted design parameters such as scale, mass, volume, texture, color and line, which are pleasing and interesting to the reasonable observer.
  - e. Compatibility: The characteristics of different uses of activities that permit them to be located near each other in harmony and without conflict. Some elements affecting compatibility include intensity of occupancy as measured by dwelling units per acre; floor area ratio; pedestrian or vehicular traffic generated; parking required; volume of goods handled; and such environmental effects as noise, vibration, glare, air pollution, erosion, or radiation.
  - f. Harmony: A quality which produces an aesthetically pleasing whole as in an arrangement of varied architectural and landscape elements.
  - g. Material Selection: Material selection as it relates to the evaluation standards and ease and feasibility of future maintenance.
  - h. Landscaping: All requirements set forth in chapter 11, "Landscaping and Trees", of this title. (Ord. 07-07, 3-26-2007)

## Recommendation

If the Appearance Commission approves the request for an Appearance Certificate for site, building, landscape plans, with associated waivers described herein, for a 60-unit mixed-use development proposed under Applications for Special Use and Subdivision (PC 25-07) for the property commonly known as 8500-50 Lehigh Avenue and a portion of Chestnut Street right of way petitioned for vacation in Morton Grove, Illinois, staff recommends the following conditions of approval:

1. *Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with a final landscape plan, including required Tree Protection and Tree Preservation Plans, for review and approval by the Community Development Administrator and Appearance Commission Chairperson. If the landscape plan is deemed to be inconsistent with the approved plan or has not been modified to remove any invasive or undesirable species, the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.*

2. The applicant shall install either fencing or year-round opaque screening a minimum of three feet in height within a five-foot (5') perimeter landscape bed along the west property line adjacent the FPDCC property. Any landscape installation along lot lines abutting the forest preserves must be limited to native species.
3. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final elevations and material specifications for review and approval. Final elevations and materials must be deemed consistent with the approved elevations and materials, as determined by the Community Development Administrator and Appearance Commission Chairperson. If such designs are deemed to be inconsistent with the approved plans or if materials are deemed to be of a lower quality than the approved materials, then the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.
4. Prior to filing any Building Permit Application, the owner/applicant shall provide the Village with final outdoor seating area plans for review and approval. Final seating arrangements, screening, and furniture specifications must be deemed consistent with the overall development, as determined by the Community Development Administrator and Appearance Commission Chairperson. If such designs are deemed to be inconsistent with the approved plans or if materials are deemed to be of a lower quality than the approved materials, then the owner/applicant will be required to file an application for an amendment to the Appearance Certificate.
5. If planter boxes or containers are provided, they must be fully planted with live vegetation when the outdoor seating area is in active use. When the outdoor seating area is not in active use, the planter boxes or containers must be (1) planted with an alternative seasonal decoration, (2) covered, or (3) removed.
6. Only frosted glass or a similar type treatment shall be used for window areas permitted to be obscured, such as fitness and service areas. The glass should not be obscured with any mirrored coating, vinyl applique, artwork, or signage.
7. Before any Building Permit Application is filed, a revised sign detail shall be provided indicating a two-foot (2') landscape bed surrounding each proposed monument sign including plantings three feet (3') in height, subject to review and approval by the Community Development Administrator.
8. All ground monument and pylon signs shall be located in a landscaped bed that extends at least two feet (2') from the base on all sides. The landscape bed of a pylon sign shall be planted with shrubs at least three feet (3') in height at planting and may also include perennials, turf or other live ground cover.
9. Sign colors shall blend with the building and storefront colors through use of complementary color ranges, or as otherwise approved by the Appearance Commission Chairperson.
10. Any portable signage shall be permitted pursuant to Section 10-10-8:E, except that the signage frame and base shall be constructed primarily of metal or wood, or as otherwise authorized by the Appearance Commission Chairperson.
11. Illuminated signage and other illuminating features on the property may not exceed 5,000K (degrees Kelvin).
12. Before any Building Permit Application is filed, the lighting plan must be revised to meet all lighting requirements of the Village of Morton Grove. The lighting plan and fixtures should also comply with all standards established in IDA's lighting guidelines and any exterior lighting should be designed to minimize the amount of light entering into the forest preserves, to the best extent practicable.
13. To mitigate bird collisions with the buildings' window area, the development must adhere to bird-friendly design guidelines contained in the "Bird-Friendly Building Design" manual of the American Bird Conservancy (2015, [https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide\\_2015.pdf](https://abcbirds.org/wp-content/uploads/2015/05/Bird-friendly-Building-Guide_2015.pdf)) where practicable. Mirrored coatings may not be used, but inconspicuous window films featuring simple dot or lined patterns are strongly encouraged.
14. [Any other condition(s) deemed appropriate by the Appearance Commission]

Attachment B  
Plan Review Comment Form for PC 25-07,  
Prepared by Amit Shah, Traffic Safety Commission Chair  
Dated September 5, 2025

REVIEWING:

**BUILDING**

**FIRE**

POLICE

**PUBLIC WORKS/ENGINEERING**

**TSC**

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VILLAGE OF MORTON GROVE, ILLINOIS  
**PLAN REVIEW COMMENT FORM**

**DATE DISTRIBUTED:** 8/15/2025

**CASE NUMBER:** PC 25-07

**APPLICATION:** Request for approval of Special Use Permits for a 57-unit mixed-use development with ground floor commercial space in a C/R Commercial/Residential District (12-4-3:D) with variations for rear yard impermeable coverage (12-2-5:B.3), setback for open accessory parking spaces and balconies (12-2-6:G), location of outdoor seating areas in a public right of way (12-5-5:C), facade transparency (12-5-7:A.3.k), dwelling units per acre (12-5-7:C), residential unit location (12-5-7:D.1), and parking lot screening abutting private property (12-11-3:B.2) for the property commonly known as 8500-8550 Lehigh Avenue in Morton Grove, Illinois.

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A Special Permit Application has been submitted to the Plan Commission for action. Please return your review to the Department of Community and Economic Development by **Friday, September 5, 2025**.

Thank you,  
Brandon Nolin, AICP  
Community Development Administrator

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**COMMENTS OR CONCERNS**

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1. Contact Metra to evaluate whether any changes to signal timing and gating are needed due to increased traffic related to the proposed development.
2. Developer should coordinate with the Village Engineer to properly locate crosswalks and include signage or traffic control such as a signal to ensure proposed crosswalks across Lehigh Avenue are safe for pedestrians.

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These comments accurately represent existing Village regulations or policies.

Name (please print): Amit Shah, Traffic Safety Commission Chairman

Signed: 

Date: 9/5/2025

Attachment C  
Plan Review Comment Forms for PC 25-07  
Prepared by Department Heads

REVIEWING:

BUILDING

FIRE

POLICE

PUBLIC WORKS/ENGINEERING

TSC

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VILLAGE OF MORTON GROVE, ILLINOIS  
PLAN REVIEW COMMENT FORM

DATE DISTRIBUTED: 6/25/2025

CASE NUMBER: PC 25-07

APPLICATION: Request for approval of Special Use Permits for a 60-unit mixed-use development with a restaurant and café in a C/R Commercial/Residential District (12-4-3:D) with variations for rear yard impermeable coverage (12-2-5:B.3), setback for open accessory parking spaces and balconies (12-2-6:G), location of outdoor seating areas in a public right of way (12-5-5:C), facade transparency (12-5-7:A.3.k), dwelling units per acre (12-5-7:C), residential unit location (12-5-7:D.1), parkway trees (12-11-2:B.4), and parking lot screening abutting private property (12-11-3:B.2) for the property commonly known as 8500-8550 Lehigh Avenue in Morton Grove, Illinois.

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A Special Permit Application has been submitted to the Plan Commission for action. Please return your review to the Department of Community and Economic Development by Friday, July 11, 2025.

Thank you,  
Brandon Nolin, AICP  
Community Development Administrator

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COMMENTS OR CONCERNS

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The Fire Department would be concerned with tight clearances to the rear of the building for emergency access. With the amount of parking spaces this could further hinder our access if numerous vehicles were trying to exit the only pathway during an emergency.

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These comments accurately represent existing Village regulations or policies.

Name (please print): Dennis Kennedy

Signed: *Dennis Kennedy*

Date: July 10, 2025

## RE: 8500-50 Lehigh Comment Form

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**From** Chris Tomich <ctomich@mortongroveil.org>

**Date** Mon 6/30/2025 12:23 PM

**To** Brandon Nolin <bnolin@mortongroveil.org>; James English <jenglish@mortongroveil.org>; Rick Dobrowski <rdobrowski@mortongroveil.org>

**Cc** Anne Ryder Kirchner <akirchner@mortongroveil.org>; Michael Lukich <mlukich@mortongroveil.org>

Brandon,

I did not review all 209 pages of the submittal, so my comments may reflect some knowledge gaps.

Fundamentally, the Village wants no proposed public infrastructure on this property. Existing public infrastructure will need to be reevaluated for abandonment to the extent practical. The existing infrastructure within the site is aged and should be renewed to the extent possible.

The proposed emergency overland flow path will need to be directed toward Lehigh Avenue.

I have concerns about reporting a lower traffic count from a larger residential development. It is intuitive that nearly all retail/commercial trips will happen by car and the previous development showed that when compared with the new development. TOD is expected to produce less trips, but that is highly dependent on the TOD location and purchasers. Some "error analysis" should be performed for the new development, unless the developer can control the number of vehicles residents will own. See traffic study narrative below. The information provided is too voluminous for me to review everything. The developer should acknowledge high reliance on the previous development and provide a solid reassessment of the original development and clearly identify the changes and the affects/effects of those changes.

### **Development Traffic Impact Comparison**

Per information published in the Highway Capacity Manual (HCM) and as described in the January 2022 traffic study, Lehigh Avenue has a capacity of about 10,000 vehicles per day (vpd), while the street currently carries about 2,950 vpd based on average daily traffic data from IDOT. Based on the AADT on Lehigh Avenue near the site and the daily trip projections for the original and new development plans, the impact to Lehigh Avenue is summarized below:

- Original Development Plan: 937 new daily trips \ ~3,890 vpd on Lehigh Avenue
- New Development Plan: 751 new daily trips \ ~3,700 vpd on Lehigh Avenue (*5% lower*)

The approximately 3,700 vpd on Lehigh Avenue is anticipated to be accommodated given the roadway's daily capacity of 10,000 vpd.

I would push them on the TOD successes their promotional literature provides. The Reserve and Drake Mixed Use in Glenview may be good TOD with longevity on its side. Let us get them to do a reassessment of what they projected for those developments and what the traffic counts look like now.

The turning path plans should provide better clearances for the Fire Department. The previously approved development had a straight emergency access route between the two buildings. This route has been eliminated the redundant route.

The proposed storm and proposed sanitary shown in the utility plan sheet need to be separated into a storm manhole and sanitary manhole for a future time when the combined sewer is separated into storm and sanitary

sewers.

The sanitary sewer is begging for refinement. First, the Moose building sewer should be realigned directly to Lehigh Avenue and be maintained by the Moose as a private sewer. More engineering work needs to be done with the feasibility of the Moose realignment. Second, the need for a grease basin could be reassessed. If needed, the sewer alignment is acceptable. If the grease basin could be eliminated, then the building sewer could be shortened.

The existing Chestnut Avenue sanitary sewer needs to be reevaluated because there is no known domestic waste upstream of the dead end of Chestnut. The developer would be responsible to determine upstream connections and abandon, if possible.

This is a tight site with a single entrance (emergency entrance eliminated by proposed building changes) in a high profile location. We need a higher level of confidence of success than at other locations within the Village. This is the reason for my concern between what is promised in an application and what is developed.

The existing conditions for the development should illustrate the train station improvements currently being constructed. The traffic study and proposed plans should be modified to incorporate all of the contracted train station improvements (entrance, crosswalks, lighting, etc.) as part of the initial submittal.

The Village should engage with the applicant with regard to public improvements within the Lehigh Avenue ROW as part of the initial application. The utility burial, streetscape, pavement improvements, etc. should be coordinated internally and articulated to the applicant.

Chris

Attachment D  
Final Plans and Supporting Documents for PC 25-07

1. *Subdivision Application, submitted by 8500 MG, LLC, received August 11, 2025*
2. *Project Narrative, submitted by 8500 MG, LLC, received August 11, 2025*
3. *Legal Description, submitted by 8500 MG, LLC, received August 11, 2025*
4. *Plat of Survey, prepared by Terra Technology Land Surveying, dated December 28, 2021*
5. *Preliminary Plat of Subdivision, prepared by Terra Technology Land Surveying, dated December 28, 2021*
6. *Special Use Application, submitted by 8500 MG, LLC, received August 11, 2025*
7. *Legal Description, submitted by Midwest RE Acquisitions, LLC, received May 12, 2025*
8. *Project Narrative, submitted by 8500 MG, LLC, received August 11, 2025*
9. *Site Context, prepared by BSB Design, dated August 7, 2025*
10. *Site Illustrative Plan, prepared by BSB Design, dated August 5, 2025*
11. *Floor Plates, prepared by BSB Design, dated August 5, 2025*
12. *Conceptual Unit Plans, prepared by BSB Design, dated August 5, 2025*
13. *Exterior Elevations, prepared by BSB Design, dated August 11, 2025*
14. *Site Plan, prepared by BSB Design, revised August 26, 2025*
15. *Conceptual Landscape Plan, prepared by RWG Engineering, revised August 26, 2025*
16. *Conceptual Landscape Details, prepared by BSB Design, revised August 5, 2025*
17. *Preliminary Site Geometric And Paving Plan, prepared by RWG Engineering, prepared August 8, 2025*
18. *Preliminary Grading Plan, prepared by RWG Engineering, prepared August 8, 2025*
19. *Preliminary Utility Plan, prepared by RWG Engineering, prepared August 8, 2025*
20. *Fire Truck Maneuvering Plan, prepared by RWG Engineering, prepared August 8, 2025*
21. *Garbage Truck Maneuvering Plan, prepared by RWG Engineering, prepared August 8, 2025*
22. *Photometric Analysis, prepared by A+M, prepared July 31, 2025*
23. *Traffic and Parking Impact Study, prepared by Kimley-Horn and Associates, Inc., dated August 8, 2025*

# SUBDIVISION APPLICATION



## Village of Morton Grove

Department of Community Development

6101 Capulina Avenue Morton Grove, Illinois 60053

(847)470-5231 (p) (847)965-4162 (f)

CASE NUMBER: \_\_\_\_\_ DATE APPLICATION FILED: \_\_\_\_\_

### APPLICANT INFORMATION

Applicant Name: \_\_\_\_\_ 8500 MG, LLC \_\_\_\_\_

Applicant Organization: \_\_\_\_\_ C/O Simon Berger \_\_\_\_\_

Applicant Address: \_\_\_\_\_ 5215 Old Orchard Rd, Suite 130 \_\_\_\_\_

Applicant City / State / Zip Code: \_\_\_\_\_ Skokie, IL 60077-1098 \_\_\_\_\_

Applicant Phone: Work: (\_\_\_\_\_) \_\_\_\_\_ Home: (\_\_\_\_\_) \_\_\_\_\_

Mobil / Other: (\_\_\_\_\_) \_\_\_\_\_

Applicant Fax: Work :(\_\_\_\_\_) \_\_\_\_\_ Home :(\_\_\_\_\_) \_\_\_\_\_

Applicant Email: \_\_\_\_\_ [REDACTED] \_\_\_\_\_

Applicant Relationship to Property Owner: \_\_\_\_\_ Future contract purchaser/developer \_\_\_\_\_

Applicant Signature: \_\_\_\_\_  \_\_\_\_\_

### PROPERTY OWNER INFORMATION (IF DIFFERENT FROM APPLICANT)

Owner Name: \_\_\_\_\_ Village of Morton Grove \_\_\_\_\_

Owner Address: \_\_\_\_\_ 6101 Capulina Avenue \_\_\_\_\_

Owner City / State / Zip Code: \_\_\_\_\_ Morton Grove, IL 60053 \_\_\_\_\_

Owner Phone: Work: ( 847 ) \_\_\_\_\_ 663-3063 \_\_\_\_\_ Home: (\_\_\_\_\_) \_\_\_\_\_

Mobil / Other: (\_\_\_\_\_) \_\_\_\_\_

Owner Fax: Work :(\_\_\_\_\_) \_\_\_\_\_ Home :(\_\_\_\_\_) \_\_\_\_\_

Owner Email: \_\_\_\_\_

Owner Signature: \_\_\_\_\_

### PROPERTY INFORMATION

Common Address of Property: \_\_\_\_\_ 8500-8550 Lehigh Avenue \_\_\_\_\_

Property Identification Number (PIN): \_\_\_\_\_ See attached \_\_\_\_\_

Legal Description (Attach additional sheets as necessary): See attached legal description.

Provide responses to the Subdivision standards as listed in Section 12-16-4.D.3 of the Unified Development Code. The Subdivision standards are as follows:

a. Orderly Development: The proposed subdivision will encourage orderly and harmonious development within the Village.

The development of a high quality project on under-utilized land will encourage orderly and harmonious development within the Village consistent with the Village's goals.

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b. Coordination of Streets: The streets within the proposed subdivision will coordinate with other existing and planned streets.

The development allows the consolidation of two Village parcels bisected by Chestnut Avenue, thereby creating an efficient development of the land which will not impact other existing and planned streets. With the vacation of Chestnut, access from Lehigh to the adjacent parcel will be by way of a cross-access easement.

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c. Coordination of Utilities: The utilities within the proposed subdivision will coordinate with other existing and planned utilities, and create a uniform system of utilities within the Village.

The development is designed to coordinate and enhance existing and planned utilities within the Village.

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d. Consistency with Comprehensive Plan: The proposed subdivision will be evaluated based on its consistency with the overall land use policies of the Village as may be expressed in the Village's comprehensive plan.

The site is designated for mixed-use and specifically calls for the promotion of large-scale mixed use development with residential and commercial uses.

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## **Station 58-Hundred**

**8500-8550 Lehigh Avenue**

**Morton Grove, Illinois**

### **Project Narrative**

The Applicant, 8500 MG, LLC is seeking special use approval, subdivision approval and right-of-way vacation approval in order to redevelop the Village-owned subject property with a mixed-use development. The site consists of an approximately 1.520 acre property located on the north and south sides of Chestnut Street, west of Lehigh Avenue. The Applicant is seeking to vacate the portion of Chestnut Avenue adjacent to both parcels. Upon vacation, the development parcel will total 2.05 acres. The site is just west of the METRA station located on Lehigh Avenue. St Paul Woods is directly west of the subject site.

The Applicant proposes to construct a 60-unit, four-story mixed-use elevator building that includes residential multi-family units, commercial space, parking and amenities. The residences will consist of one- and two-bedroom units with high-end finishes and multiple floor plan options. The residential amenities include an entry lounge, co-work area, a fully equipped fitness center and an outdoor terrace facing Lehigh Avenue. The proposed building will feature over 4,000 sq feet of commercial space on the first floor, with outdoor patio space.

This prime location on Lehigh Avenue west of the Metra Station allows for an opportunity to extend Morton Grove's Downtown neighborhood west to encompass the Lehigh Avenue corridor and Metra Station. Chestnut Street currently bisects the site and it is proposed to be vacated with access to the Moose Family Center to be adjusted to the south side of the site. The overhead wires will be placed underground, and the Chestnut Street utilities will be relocated.

The mixed-use building is designed to create a dynamic streetscape along Lehigh, incorporating outdoor dining areas and the residential entrance and terrace facing east towards the historic downtown and Gateway Park. The parking area on the west side of the site is shielded by the building and a valet/drop-off area is proposed along the west side of Lehigh along with the potential of on-street parking to provide additional retail parking. The units will all have balconies, and the building façade materials will be brick, cultured stone and cementitious panels creating a classic look that will enhance the surrounding architecture. The building will be approximately 54' tall with the first-floor height expanded to meet the requirements of the commercial and fitness areas and 9' ceilings on the upper residential floors.

## LEGAL DESCRIPTION

THE SOUTH 120 FEET OF LOTS 6, 7, 8, 9, 10, AND 11 IN BLOCK 1 IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE EAST 10 ACRES OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS

LOTS 12 IN BLOCK 1 EXCEPTING THEREFROM THAT PART LYING EAST OF A LINE DRAWN PARALLEL WITH THE WEST LINE OF LOT 12, SAID LINE BEING 34 FEET (AS MEASURED ALONG THE SOUTH LINE OF LOT 12) WEST OF THE INTERSECTION OF THE WESTERLY LINE OF LEHIGH AVE. AND THE NORTH LINE OF CHESTNUT ST. IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE EAST 10 ACRES OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS

LOTS 3, 4, AND 5 IN BLOCK 2 IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE EAST 10 ACRES OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS

Containing 1.520 +/-

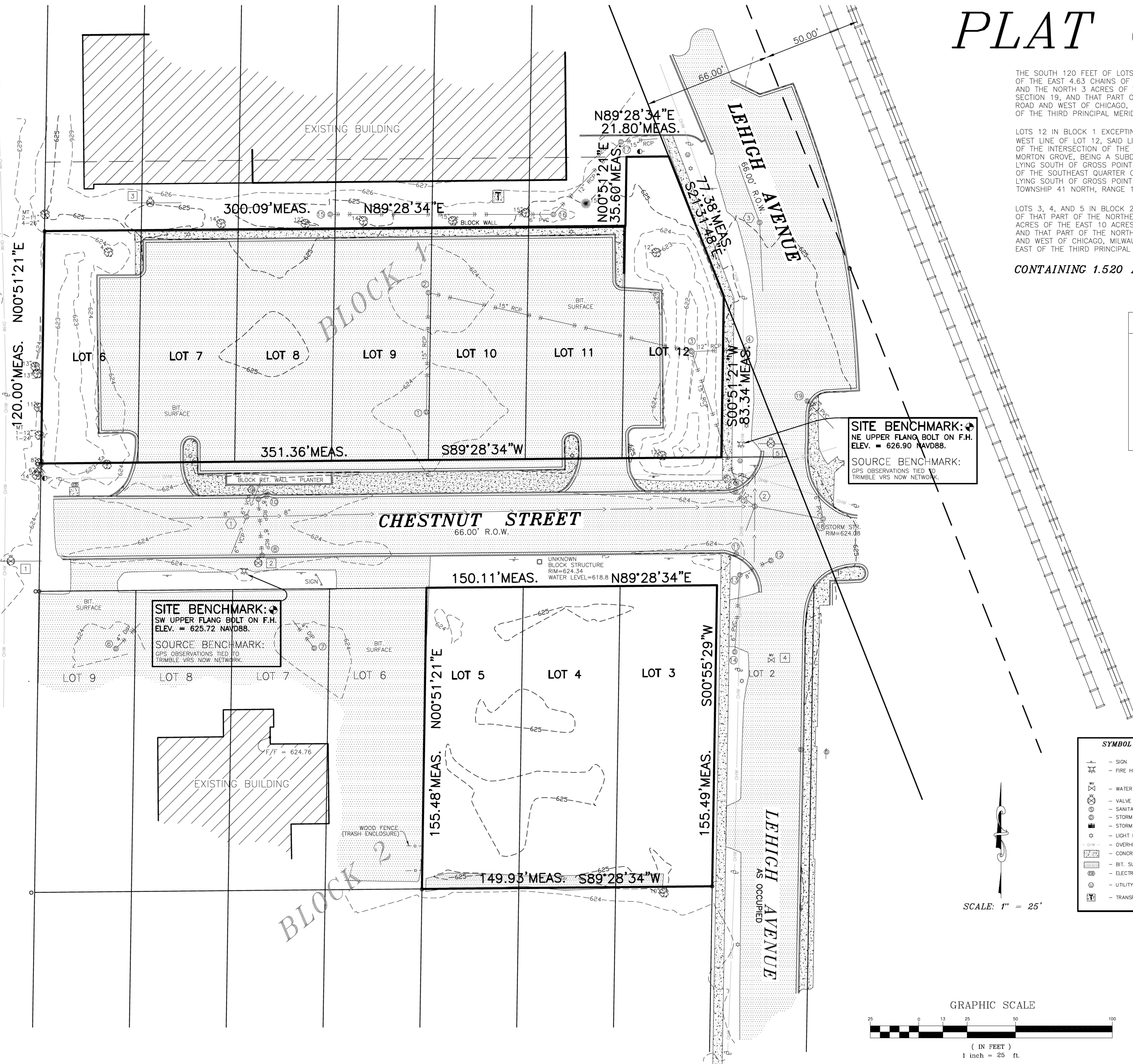
# PLAT OF SURVEY

THE SOUTH 120 FEET OF LOTS 6, 7, 8, 9, 10, AND 11 IN BLOCK 1 IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE EAST 10 ACRES OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

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CONTAINING 1.520 ACRES +/-



**SITE BENCHMARK:** NE UPPER FLANG BOLT ON F.H. ELEV. = 626.90 NAVD88.

**SOURCE BENCHMARK:** GPS OBSERVATIONS TIED TO TRIMBLE VRS NOW NETWORK.

**SITE BENCHMARK:** SW UPPER FLANG BOLT ON F.H. ELEV. = 625.72 NAVD88.

**SOURCE BENCHMARK:** GPS OBSERVATIONS TIED TO TRIMBLE VRS NOW NETWORK.

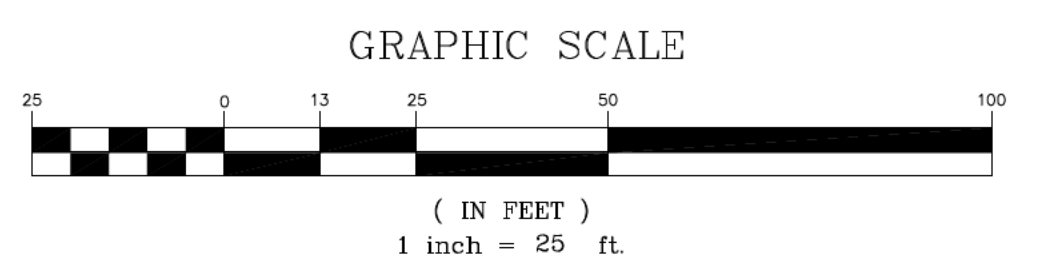
UTILITY SCHEDULED - SANITARY SEWER	
1 SAN MH RIM=623.80 I(E-W)=618.00 I(SW)=618.85 I(NE)=618.75	3 SAN MH RIM=625.08 I(N-S)=614.28
2 SAN MH RIM=624.31 I(N-S)=614.61 I(NW)=617.96 I(E)=618.86 I(SW)=619.21 I(W)=616.16	

UTILITY SCHEDULED - WATER MAIN	
1 VALVE VAULT RIM=624.68 T-W=619.4	4 WATER VALVE RIM=624.54
2 VALVE VAULT (PROBABLE) RIM=623.60 T-W=618.4	5 VALVE VAULT RIM=624.17 T-W=620.0 FULL OF WATER
3 VALVE VAULT RIM=625.09 T-W=619.2	

UTILITY SCHEDULED - STORM SEWER	
1 C.B. RIM=623.56 I(N)=619.47	10 INLET RIM=623.36 I(SW-6" PVC)=621.27
2 RIM=623.61 I(SE)=619.16 I(S)=619.21	11 C.B. RIM=623.76 WATER=620.26 NO VISIBLE PIPES
3 C.B. RIM=622.32 I(SE)=618.70 I(E)=618.72 I(NW)=618.72	12 STORM STR. RIM=624.50
4 C.B. RIM=623.80 I(SE)=619.04	13 M.H. RIM=624.30 I(NE)=620.30 I(S)=621.66
5 RESTR. STR. RIM(N)=623.93 RIM(S)=623.86 I(SE)=618.18 I(NW)=618.15 I(4" REST)=618.21 T(WER)=621.23	14 INLET RIM=624.26 I(N)=622.66
6 INLET RIM=623.68 I(NE)=620.43	15 STORM STR. RIM=624.07 FULL OF SILT
7 INLET RIM=623.52 I(NW)=620.00	16 INLET RIM=623.44 I(NE)=619.34 I(W)=619.34
8 INLET RIM=623.23 I(N)=621.33	17 M.H. RIM=626.28 I(E)=618.36 I(SW)=618.38
9 C.B. RIM=623.42 I(NE-6" PVC)=621.02 I(S)=620.10 WATER LEVEL=620.42	18 M.H. RIM=624.08 I(N)=621.03 I(W) COULD NOT OPEN
	19 INLET RIM=624.24 I(SE)=623.29

SYMBOL LEGEND	
[Symbol]	SIGN
[Symbol]	FIRE HYDRANT
[Symbol]	WATER VALVE
[Symbol]	VALVE VAULT
[Symbol]	SANITARY STRUCTURE
[Symbol]	STORM STRUCTURE
[Symbol]	STORM STRUCTURE (CURB LINE)
[Symbol]	LIGHT POLE
[Symbol]	OVERHEAD WIRES
[Symbol]	CONCRETE SURFACE
[Symbol]	BIT SURFACE
[Symbol]	ELECTRIC CONTROL BOX
[Symbol]	UTILITY CANISTER
[Symbol]	TRANSFORMER

SCALE: 1" = 25'



STATE OF ILLINOIS  
COUNTY OF LAKE SS.

I, Vydas Z. Rekasius, an Illinois Professional Land Surveyor, do hereby certify that I have surveyed the property described above and that the plat shown hereon is a correct representation of said survey.

DATED THIS 10th DAY OF JANUARY, A.D. 2022.

BY: \_\_\_\_\_  
Illinois Professional Land Surveyor No. 3210  
License Renewal Date : 11/30/2022  
DESIGN FIRM NO. 184-004538 RENEWAL DATE: 4/30/2023  
REVISED: 1/11/2022 - LOT 12 BOUNDARY

**SURVEY PERFORMED WITHOUT REFERENCE TO TITLE COMMITMENT**

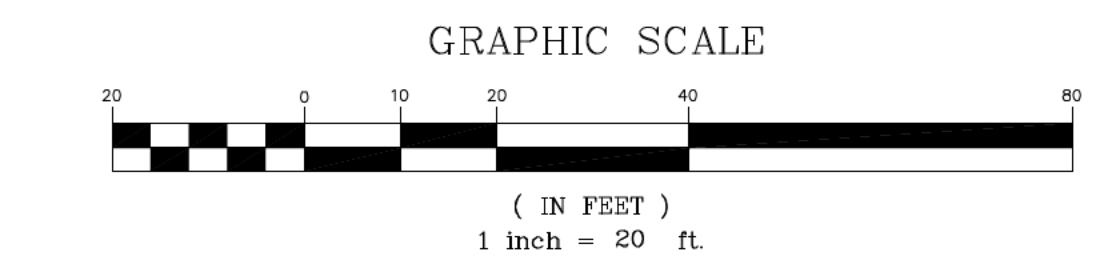
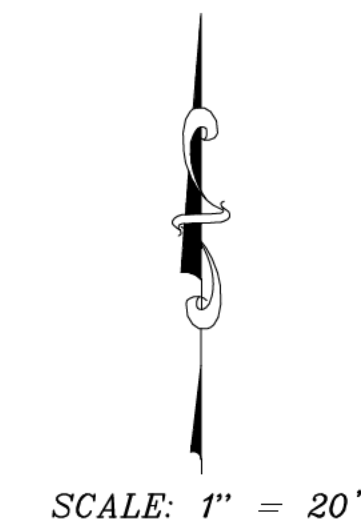
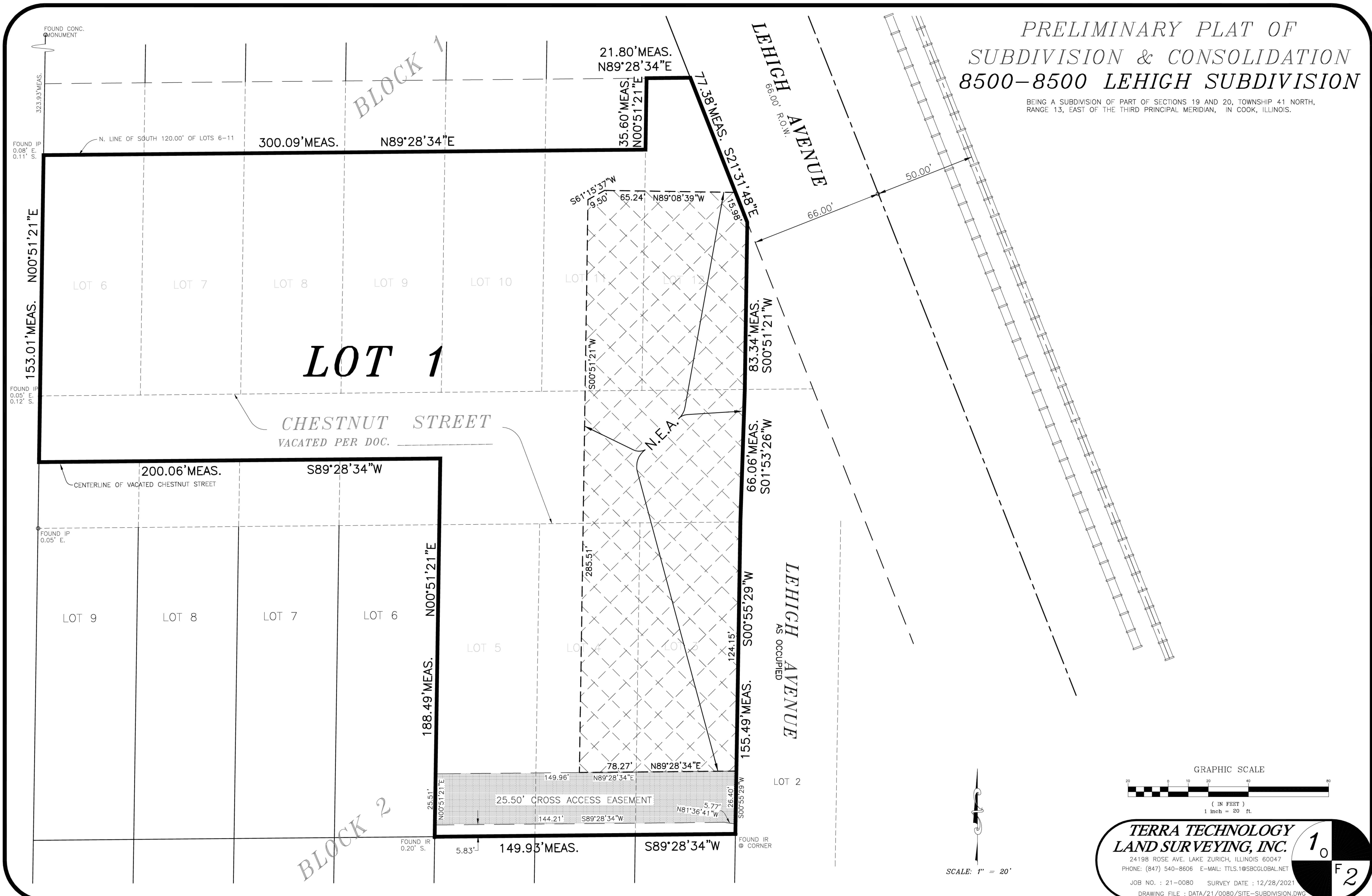
NOTE:  
1. Please check Legal Description with Deed and report any discrepancy immediately.  
2. Compare all points before building by same and report any discrepancies at once.  
3. Building lines, if any, shown hereon are building lines shown on the recorded subdivision plat or called out in the title report.  
4. Consult local authorities for building lines established by local ordinances.  
5. This professional service conforms to the current Illinois minimum standards for a boundary survey.

**TERRA TECHNOLOGY  
LAND SURVEYING, INC.**  
24198 ROSE AVE. LAKE ZURICH, ILLINOIS 60047  
PHONE: (847) 540-8606 E-MAIL: TTLS1@SBCGLOBAL.NET

JOB NO. : 21-0080 SURVEY DATE : 12/28/2021  
DRAWING FILE : DATA/21/0080/SITE-SURVEY.DWG

PRELIMINARY PLAT OF  
SUBDIVISION & CONSOLIDATION  
8500-8500 LEHIGH SUBDIVISION

BEING A SUBDIVISION OF PART OF SECTIONS 19 AND 20, TOWNSHIP 41 NORTH,  
RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK, ILLINOIS.



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10  
F2

# PRELIMINARY PLAT OF SUBDIVISION & CONSOLIDATION 8500-8500 LEHIGH SUBDIVISION

BEING A SUBDIVISION OF PART OF SECTIONS 19 AND 20, TOWNSHIP 41 NORTH,  
RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK, ILLINOIS.

## LAND SURVEYOR CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF LAKE )

I, VYDAS Z. REKASIUS, AN ILLINOIS PROFESSIONAL SURVEYOR, HEREBY CERTIFIES THAT I HAVE SURVEYED THE FOLLOWING DESCRIBED PROPERTY:

THE SOUTH 120 FEET OF LOTS 6, 7, 8, 9, 10, AND 11 IN BLOCK 1 IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE NORTH HALF OF THE SOUTH EAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

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THAT PART OF VACATED CHESTNUT STREET DESCRIBED AS FOLLOWS: BEGINNING AT THE SOUTHWEST CORNER OF LOT 6 IN BLOCK 1 IN MORTON GROVE, BEING A SUBDIVISION OF THE EAST 4.63 CHAINS OF THAT PART OF THE NORTHEAST QUARTER LYING SOUTH OF GROSS POINT ROAD AND THE NORTH 3 ACRES OF THE EAST 10 ACRES OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 19, AND THAT PART OF THE NORTHWEST QUARTER OF SECTION 20 LYING SOUTH OF GROSS POINT ROAD AND WEST OF CHICAGO, MILWAUKEE AND ST. PAUL RAILROAD, IN TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN; THENCE NORTH 89 DEGREES 28 MINUTES 34 SECONDS EAST ALONG THE NORTH LINE OF SAID CHESTNUT STREET, 351.36 FEET TO A POINT ON THE SOUTH LINE OF LOT 12 IN SAID SUBDIVISION, SAID POINT BEING 34 FEET (AS MEASURED ALONG THE SOUTH LINE OF LOT 12) WEST OF THE INTERSECTION OF THE WESTERLY LINE OF LEHIGH AVE. AND THE NORTH LINE OF CHESTNUT ST.; THENCE SOUTH 01 DEGREES 53 MINUTES 26 SECONDS WEST, 66.06 FEET TO A POINT ON THE SOUTH LINE OF SAID CHESTNUT STREET; THENCE SOUTH 89 DEGREES 28 MINUTES 34 SECONDS WEST ALONG SAID LINE, 150.11 FEET TO THE NORTH WEST CORNER OF LOT 5 IN BLOCK 2 IN SAID SUBDIVISION; THENCE NORTH 00 DEGREES 51 MINUTES 21 SECONDS EAST ALONG THE WEST LINE OF SAID LOT 5 EXTENDED NORTH, 33.01 FEET TO THE CENTERLINE OF VACATED CHESTNUT STREET; THENCE SOUTH 89 DEGREES 28 MINUTES 34 SECONDS WEST, 200.06 FEET; THENCE NORTH 00 DEGREES 51 MINUTES 21 SECONDS EAST, 33.01 FEET TO THE PLACE OF BEGINNING, CONTAINING 0.380 ACRES MOR OR LESS, ALL IN COOK COUNTY, ILLINOIS.

CONTAINING 1.900 ACRES +/-

AND THAT THE PLAT HEREON DRAWN IS A CORRECT REPRESENTATION OF SAID SURVEY.

I FURTHER CERTIFY THAT BASED UPON AN EXAMINATION OF THE FEDERAL EMERGENCY MANAGEMENT AGENCY (F.E.M.A.) FLOOD INSURANCE RATE MAP COMMUNITY-PANEL NO. 17031C0241L, SHOWS THAT THE PROPERTY SURVEYED HEREON IS NOT SUBJECT TO FLOOD RISK AND THAT SAID PROPERTY FALLS WITHIN ZONE X (AREA OUTSIDE OF 0.2% ANNUAL CHANCE FLOODPLAIN).

(I FURTHER CERTIFY THAT THE LAND INCLUDED BY SAID SURVEY IS WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF MORTON GROVE, COOK COUNTY, ILLINOIS, WHICH HAS ADOPTED A COMPREHENSIVE PLAN AND IS EXERCISING ITS EXTRATERRITORIAL POWER AS AUTHORIZED PURSUANT TO AND IN ACCORDANCE WITH SEC. 11-12-5, 65 ILCS 5/11-12-5, OF THE ILLINOIS MUNICIPAL CODE AND ITS HOME RULE POWERS)

I FURTHER CERTIFY THAT IRON PIPE SURVEY STAKES OR THE CONCRETE MONUMENTS AS SHOWN ON THE PLAT HEREON DRAWN, HAVE BEEN ESTABLISHED AT THE LOT CORNERS.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, 2025.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-003210

License Renewal Date : 11/30/2026  
DESIGN FIRM NO. 184-004538  
RENEWAL DATE: 4/30/27



REV: 8/6/25 - NEA

**TERRA TECHNOLOGY  
LAND SURVEYING, INC.**

24198 ROSE AVE. LAKE ZURICH, ILLINOIS 60047  
PHONE: (847) 540-8606 E-MAIL: TTLS.1@SBGGLOBAL.NET

JOB NO. : 21-0080 SURVEY DATE : 12/28/2021

DRAWING FILE : DATA/21/0080/SITE-SUBDIVISION.DWG

## OWNER'S CERTIFICATE

STATE OF \_\_\_\_\_ )  
) ss.  
COUNTY OF \_\_\_\_\_ )

\_\_\_\_\_ DOES HEREBY CERTIFY THAT IT IS, [AS SUCH TRUSTEE], TITLE HOLDER OF THE PROPERTY DESCRIBED HEREON; DOES HEREBY ACKNOWLEDGE AND ADOPT THE SAME UNDER THE STYLE AND TITLE HEREON SHOWED; HEREBY DEDICATES THE PUBLIC ROADS, STREETS, ALLEYS, WALKS, AND OTHER AREAS INDICATED HEREON FOR PUBLIC USE; AND ESTABLISHES AND GRANTS ANY OTHER EASEMENTS SHOWN HEREON. [INSERT NAME OF OWNER, OWNER ENTITY, TRUST] FURTHER CERTIFIES TO THE BEST OF ITS KNOWLEDGE, THAT THE LAND INCLUDED IN THE PLAT HEREON DRAWN FALLS WITH SCHOOL DISTRICTS:

DATED AT \_\_\_\_\_, ILLINOIS, THIS DAY OF \_\_\_\_\_, 2025.

BY: \_\_\_\_\_ TITLE: \_\_\_\_\_

ATTEST: \_\_\_\_\_ TITLE: \_\_\_\_\_

## NOTARY'S CERTIFICATE

STATE OF \_\_\_\_\_ )  
) ss.  
COUNTY OF \_\_\_\_\_ )

I, [INSERT NAME OF NOTARY], A NOTARY PUBLIC IN AND FOR SAID COUNTY IN THE STATE OF \_\_\_\_\_, DO HEREBY CERTIFY THAT [INSERT NAME OF FIRST SIGNATOR ABOVE] AND [INSERT NAME OF ATTEST SIGNATOR ABOVE] OF [INSERT NAME OF OWNER, OWNER ENTITY, TRUST], PERSONALLY KNOWN TO ME TO BE THE SAME PERSONS WHOSE NAMES ARE SUBSCRIBED TO THE FOREGOING INSTRUMENT AS [INSERT TITLE OF FIRST SIGNATOR ABOVE] AND [INSERT TITLE OF ATTEST SIGNATOR ABOVE], RESPECTIVELY, APPEARED BEFORE ME THIS DAY IN PERSON AND ACKNOWLEDGED THAT THEY SIGNED AND DELIVERED THE SAID INSTRUMENT AS THEIR OWN FREE AND VOLUNTARY ACT AND AS THE FREE AND VOLUNTARY ACT OF SAID [INSERT NAME OF OWNER, OWNER ENTITY, TRUST] FOR THE USES AND PURPOSES THEREIN SET FORTH.

GIVEN UNDER MY HAND AND NOTARIAL SEAL THIS \_\_\_ DAY OF \_\_\_\_\_, 2025.

## SURFACE WATER DRAINAGE CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

TO THE BEST OF OUR KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATER WILL NOT BE CHANGED BY THE CONSTRUCTION OF THIS SUBDIVISION OR ANY PART THEREOF, OR, IF SUCH SURFACE WATER DRAINAGE WILL BE CHANGED, REASONABLE PROVISION HAS BEEN MADE FOR COLLECTION AND DIVERSION OF SUCH SURFACE WATERS INTO PUBLIC AREAS, OR DRAINS WHICH THE OWNER HAS THE RIGHT TO USE, AND THAT SUCH SURFACE WATERS WILL BE PLANNED FOR IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES AS TO REDUCE THE LIKELIHOOD OF DAMAGE TO THE ADJOINING PROPERTY BECAUSE OF THE CONSTRUCTION OF THIS SUBDIVISION.

BY: \_\_\_\_\_ OWNER DATED: \_\_\_\_\_

BY: \_\_\_\_\_ ILLINOIS PROFESSIONAL ENGINEER DATED: \_\_\_\_\_

## VILLAGE ENGINEER CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

APPROVED BY THE VILLAGE ENGINEER OF THE VILLAGE OF MORTON GROVE, COOK COUNTY, ILLINOIS, ON THIS \_\_\_ DAY OF \_\_\_\_\_, 2025.

\_\_\_\_\_  
VILLAGE ENGINEER

## COOK COUNTY RECORDER CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

THIS INSTRUMENT WAS FILED FOR RECORD IN THE OFFICE OF THE COOK COUNTY RECORDER OF DEEDS, ILLINOIS, ON THE \_\_\_ DAY OF \_\_\_\_\_, A.D. 2025 AT \_\_\_ O'CLOCK \_\_\_ M., AND WAS RECORDED AS DOCUMENT NO. \_\_\_\_\_.

\_\_\_\_\_  
RECORDER OF DEEDS

## PLAN COMMISSION CERTIFICATE

APPROVED BY THE CHAIRPERSON OF THE PLAN COMMISSION OF THE VILLAGE OF MORTON GROVE, COOK COUNTY, ILLINOIS, THIS \_\_\_ DAY OF \_\_\_\_\_, 2025, PURSUANT TO ORDINANCE 20-08 AND TITLE 12, CHAPTER 8, OF THE MORTON GROVE MUNICIPAL CODE (ORDINANCE 07-07).

BY: \_\_\_\_\_ CHAIRPERSON

ATTEST: \_\_\_\_\_ SECRETARY

## VILLAGE CLERK CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

I DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT SPECIAL ASSESSMENTS OR UNPAID CURRENT SPECIAL ASSESSMENTS DUE AGAINST THE LAND INCLUDED IN THE ABOVE PLAT.

DATED THIS \_\_\_ DAY OF \_\_\_\_\_, 2025.

BY: \_\_\_\_\_ VILLAGE CLERK

## VILLAGE BOARD CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

APPROVED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF MORTON GROVE, COOK COUNTY, ILLINOIS ON THIS \_\_\_ DAY OF \_\_\_\_\_, 2025.

\_\_\_\_\_  
VILLAGE PRESIDENT

\_\_\_\_\_  
VILLAGE CLERK

## COOK COUNTY CLERK CERTIFICATE

STATE OF ILLINOIS )  
) ss.  
COUNTY OF COOK )

I DO NOT FIND ANY DELINQUENT GENERAL TAXES, UNPAID CURRENT GENERAL TAXES, DELINQUENT SPECIAL ASSESSMENTS OR UNPAID CURRENT SPECIAL ASSESSMENTS AGAINST THE TRACT OF LAND IN THE ABOVE PLAT.

\_\_\_\_\_  
COUNTY CLERK

DATE: \_\_\_\_\_

## EASEMENT NOTE:

A BLANKET PUBLIC UTILITY AND DRAINAGE EASEMENT IS HERBY GRANTED OVER LOT 1, EXCEPT FOR AREAS DESIGNATED AS "N.E.A." (NON EASEMENT AREAS).

### CROSS ACCESS EASEMENT PROVISIONS

The interested parties representing Lots 1 and Lots 5,6,7, and 8 in Block 2 hereby grant and convey non-exclusive, mutual cross access easements for purposes of vehicular and pedestrian ingress and egress on, over, upon and across the areas defined in the Easement Areas. The Cross Access Easement is subject to the terms, conditions, restrictions and limitations set forth herein and in other recorded easements, reservations, rights-of-way, licenses, restrictions, conditions and limitations affecting the Easement Areas; provided, however, that the foregoing shall not unreasonably interfere with the easement rights under this Agreement. The Cross Access Easement is for the benefit of and is appurtenant to each of the Parcels, respectively, and may be used by the record title owner of each of the Lots, respectively, and each of their respective successors, assigns, employees, contractors, agents, licensees, lessees under leases extending the use thereof to such lessees and other permittees (collectively the "Permitted Users") solely for the uses set forth herein (the "Permitted Uses") and for no other uses. Such Permitted Uses shall be for the benefit of the Lots as now or hereafter improved, subdivided and/or developed.

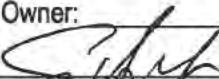


# SPECIAL USE APPLICATION

Village of Morton Grove  
Department of Community Development  
6101 Capulina Avenue, Morton Grove, Illinois 60053  
commdev@mortongroveil.org | 847-663-3063

Case Number: PC 25-07 Date Application Filed: 08/11/2025

## APPLICANT INFORMATION

Applicant Name: 8500 MG, LLC  
Applicant Organization: C/O Simon Berger  
Applicant Address: 5215 Old Orchard Rd, Suite 130  
Applicant City / State / Zip Code: Skokie, IL 6007-1098  
Applicant Phone: [REDACTED]  
Applicant Email: [REDACTED]  
Applicant Relationship to Property Owner: Future contract purchaser/developer  
Applicant Signature: 

## PROPERTY OWNER INFORMATION (IF DIFFERENT FROM APPLICANT)

Owner Name: Village of Morton Grove  
Owner Address: 6101 Capulina Avenue  
Owner City / State / Zip Code: Morton Grove, IL 60053  
Owner Phone: (847) 663-3063  
Owner Email: \_\_\_\_\_  
Owner Signature: \_\_\_\_\_

## PROPERTY INFORMATION

Common Address of Property: 8500-8550 Lehigh Avenue  
Property Identification Number (PIN): See attached  
Property Square Footage: 82,781 sq. ft. (1.90 acres)  
Legal Description (attach as necessary): See attached  
Property Zoning District: C/R Commercial Residential

## APPLICATION INFORMATION

Requested Special Use: \_\_\_\_\_  
Purpose of Special Use (attach as necessary): See attached project narrative.

**RESPONSES TO STANDARDS FOR SPECIAL USE**

Provide responses to the seven (7) Standards for Special Use as listed in Section 12-16-4-C-5 of the Village of Morton Grove Unified Development Code. The applicant must present this information for the official record of the Planning Commission. The Special Use Standards are as follows:

- a. The establishment, maintenance, or operation of the Special Use will not be detrimental to, or endanger the public health, safety, morals, comfort, or general welfare.

The project conforms to Village ordinances, will enhance the area adjacent to the train station and not be detrimental to public health, safety, morals, comfort or general welfare.

- b. The Special Use will not be injurious to the use and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood.

The project involves a significant investment in a key parcel in the Village, provides a development that will satisfy existing demand, and will not be injurious to the use and enjoyment of other property and will not diminish or impair property values in the neighborhood.

- c. The establishment of the Special Use will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the district.

The project is designed in conformance with Village codes and ordinances and will not impede the orderly development and improvement of the surrounding property for uses permitted in the district.

- d. Adequate utilities, access roads, drainage and/or necessary facilities have been or are being provided.

The project is designed with adequate utilities, access, drainage and other necessary facilities.

- e. Adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets.

The project is designed such that ingress and egress to it will minimize congestion with measures being included to ensure no adverse impact.

- f. The proposed Special Use is not contrary to the objectives of the current Comprehensive Plan for the Village of Morton Grove.

The site is designated for mixed-use and specifically calls for the promotion of large-scale mixed use development with residential and commercial uses.

- g. The Special Use shall, in all other respects, conform to the applicable regulations of the district in which it is located, except as such regulations may, in each instance, be modified pursuant to the recommendations of the Commission.

The special use will conform to the applicable regulations of the C/R Commercial Residential District.

## **Station 58-Hundred**

**8500-8550 Lehigh Avenue**

**Morton Grove, Illinois**

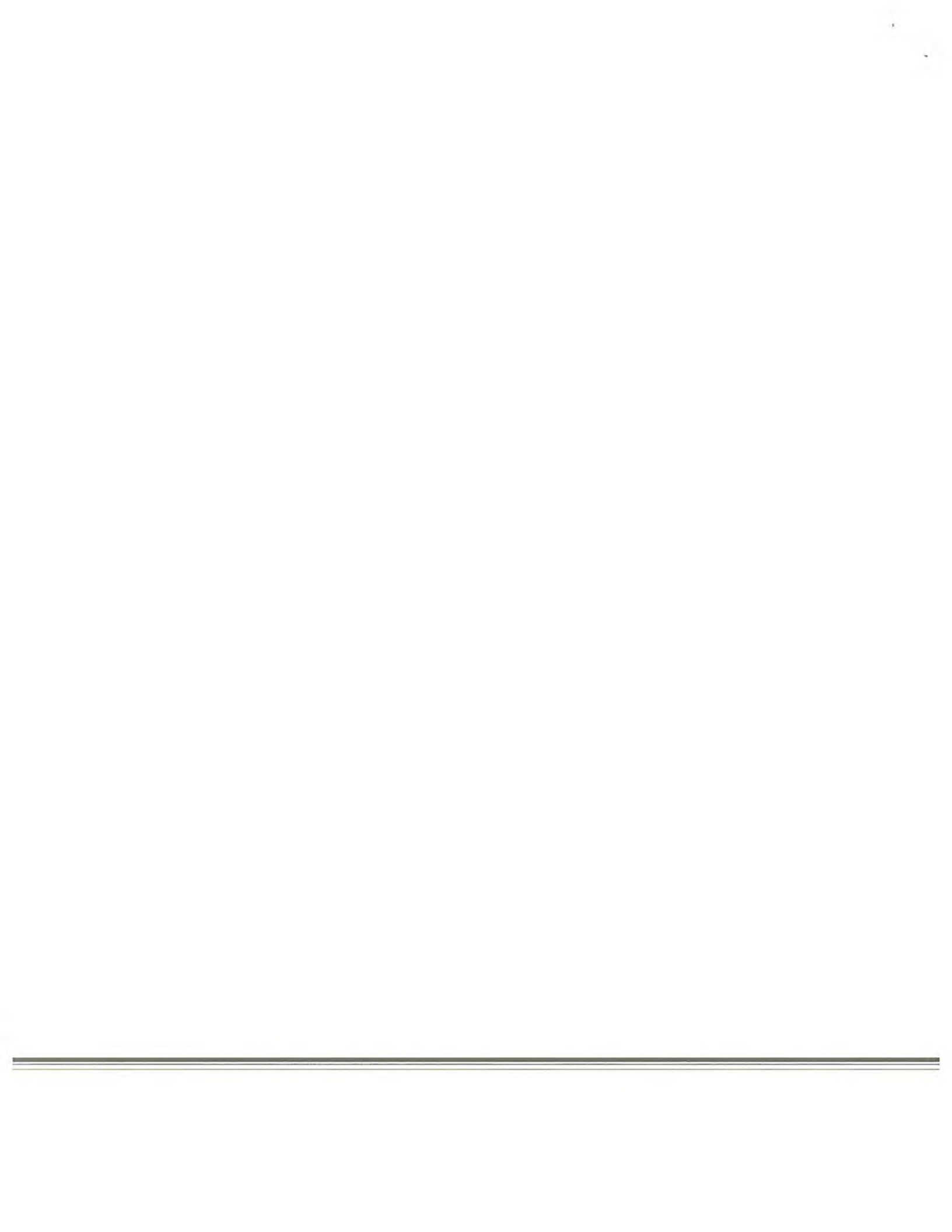
### **Project Narrative**

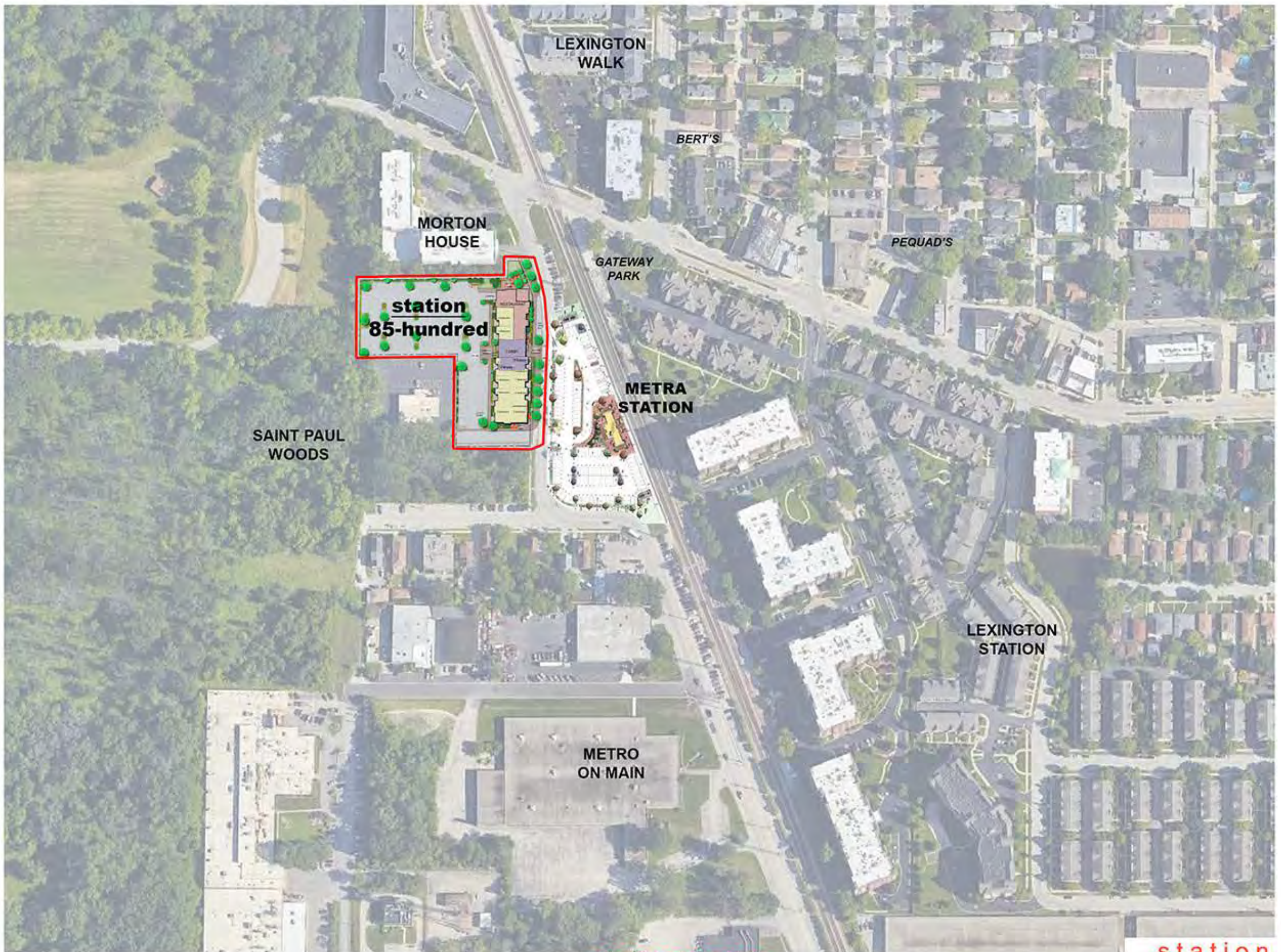
The Applicant, 8500 MG, LLC is seeking special use approval, subdivision approval and right-of-way vacation approval in order to redevelop the Village-owned subject property with a mixed-use development. The site consists of an approximately 1.520 acre property located on the north and south sides of Chestnut Street, west of Lehigh Avenue. The Applicant is seeking to vacate the portion of Chestnut Avenue adjacent to both parcels. Upon vacation, the development parcel will total 2.05 acres. The site is just west of the METRA station located on Lehigh Avenue. St Paul Woods is directly west of the subject site.

The Applicant proposes to construct a 60-unit, four-story mixed-use elevator building that includes residential multi-family units, commercial space, parking and amenities. The residences will consist of one- and two-bedroom units with high-end finishes and multiple floor plan options. The residential amenities include an entry lounge, co-work area, a fully equipped fitness center and an outdoor terrace facing Lehigh Avenue. The proposed building will feature over 4,000 sq feet of commercial space on the first floor, with outdoor patio space.

This prime location on Lehigh Avenue west of the Metra Station allows for an opportunity to extend Morton Grove's Downtown neighborhood west to encompass the Lehigh Avenue corridor and Metra Station. Chestnut Street currently bisects the site and it is proposed to be vacated with access to the Moose Family Center to be adjusted to the south side of the site. The overhead wires will be placed underground, and the Chestnut Street utilities will be relocated.

The mixed-use building is designed to create a dynamic streetscape along Lehigh, incorporating outdoor dining areas and the residential entrance and terrace facing east towards the historic downtown and Gateway Park. The parking area on the west side of the site is shielded by the building and a valet/drop-off area is proposed along the west side of Lehigh along with the potential of on-street parking to provide additional retail parking. The units will all have balconies, and the building façade materials will be brick, cultured stone and cementitious panels creating a classic look that will enhance the surrounding architecture. The building will be approximately 54' tall with the first-floor height expanded to meet the requirements of the commercial and fitness areas and 9' ceilings on the upper residential floors.





**SITE CONTEXT**

**station  
85-hundred**

8500 Lehigh Avenue  
Morton Grove, Illinois



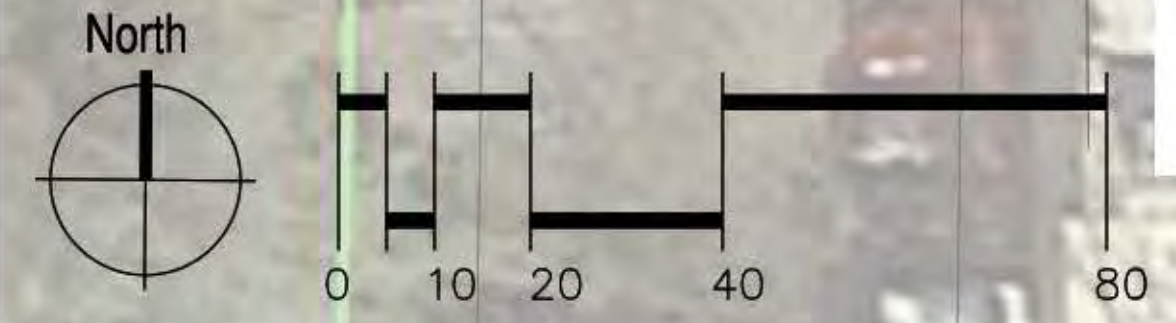
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847.208.8211  
5215 OLD ORCHARD RD, STE 130  
SKOKIE 60077



August 7, 2025

Morton House  
Condominiums

Gateway  
Park



Morton Grove  
Moose Family  
Center

Moose Lodge  
Entry Sign  
Moose Lodge  
Entrance

Moose Lodge  
Entry Sign

METRA PARKING

Site Illustrative Plan

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5215 OLD ORCHARD RD, STE 130  
SKOKIE 60077

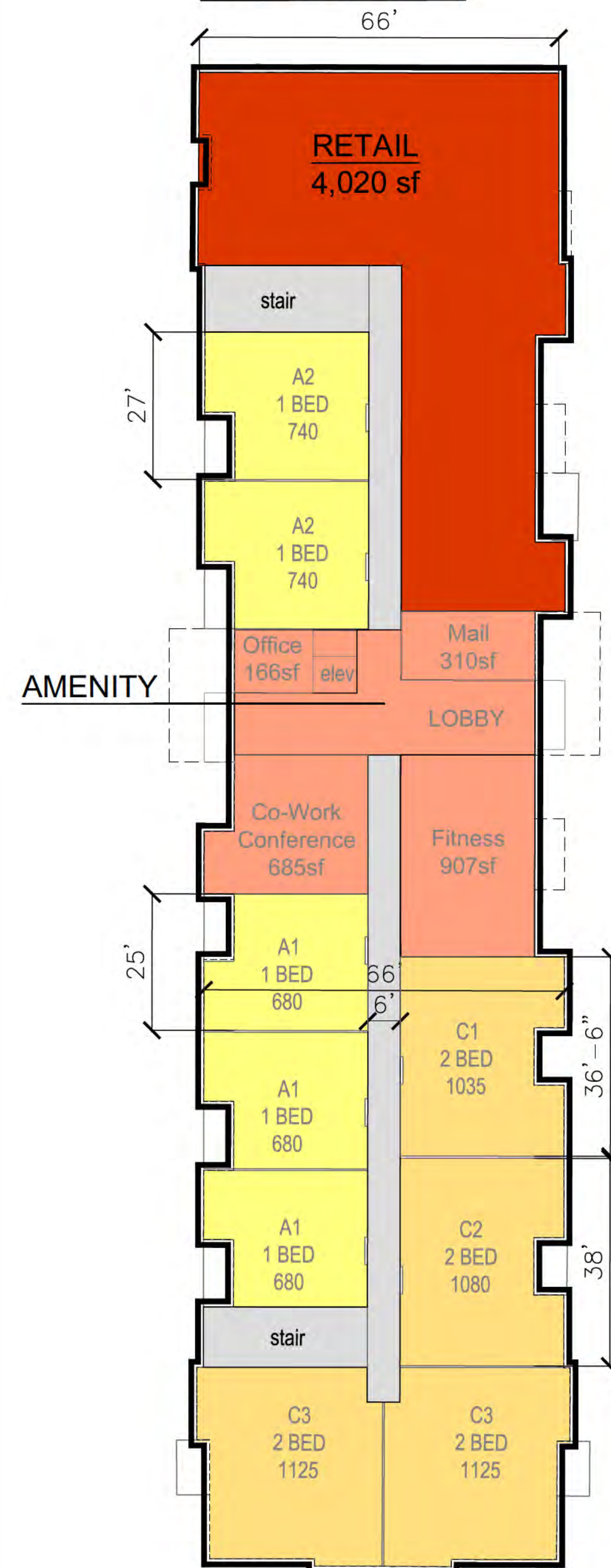
station  
85-hundred  
8500 Lehigh Avenue  
Morton Grove, Illinois

BSB  
DESIGN  
BSBDESIGN.COM

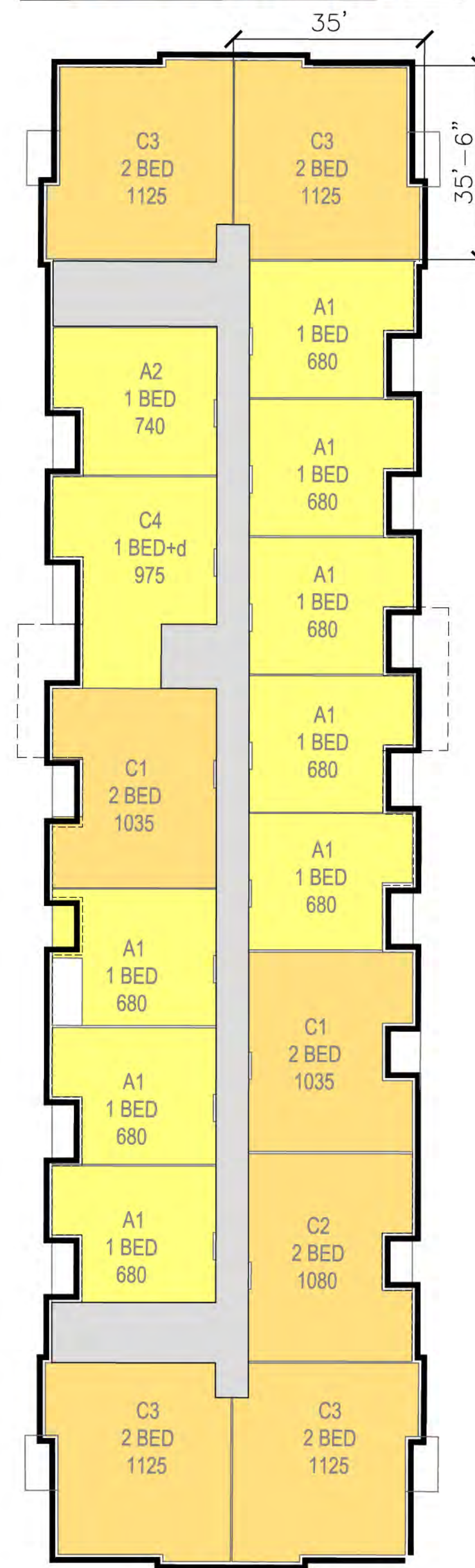
August 5, 2025

The drawings presented are illustrative of character and design intent only, and are subject to change based upon final design considerations (i.e. applicable codes, structural, and MEP design requirements, unit plan / floor plan changes, etc.) © 2025 BSB Design, Inc.

### FLOOR 1



### FLOORS 2, 3, 4



### FLOOR 1

#### 5 - 1 Bed

- 3 - A1 680 sf
- 2 - A2 740 sf

#### 4 - 2 Bed

- 1 - C1 1035 sf
- 1 - C2 1080 sf
- 2 - C3 1125 sf

### FLOORS 2, 3, 4

#### 27 - 1 Bed

- 24 - A1 680 sf
- 3 - A2 740 sf

#### 3 - 1 Bed+Den

- 3 - C4 975 sf

#### 21 - 2 Bed

- 6 - C1 1035 sf
- 3 - C2 1080 sf
- 12 - C3 1125 sf

### BUILDING INFO

Total Gross Sq Feet	67,248 sf
Common Areas (Corridor, Stair, Lobby)	8,822 sf
Amenities	2,068 sf
Retail	4,020 sf
Residential	52,338 sf

### Floor Plates



station  
85 - hundred  
8500 Lehigh Avenue  
Morton Grove, Illinois

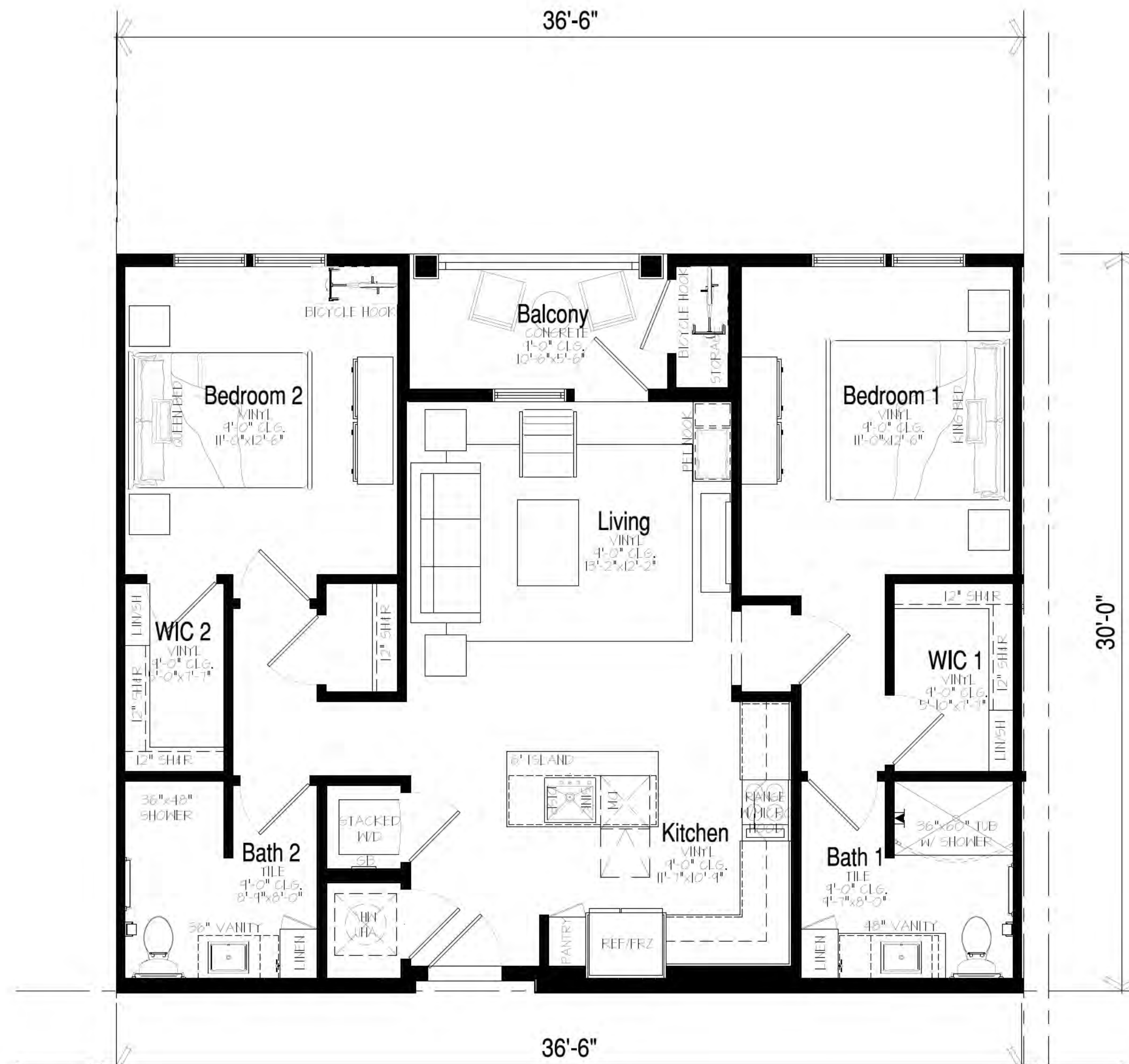


Revised August 5, 2025



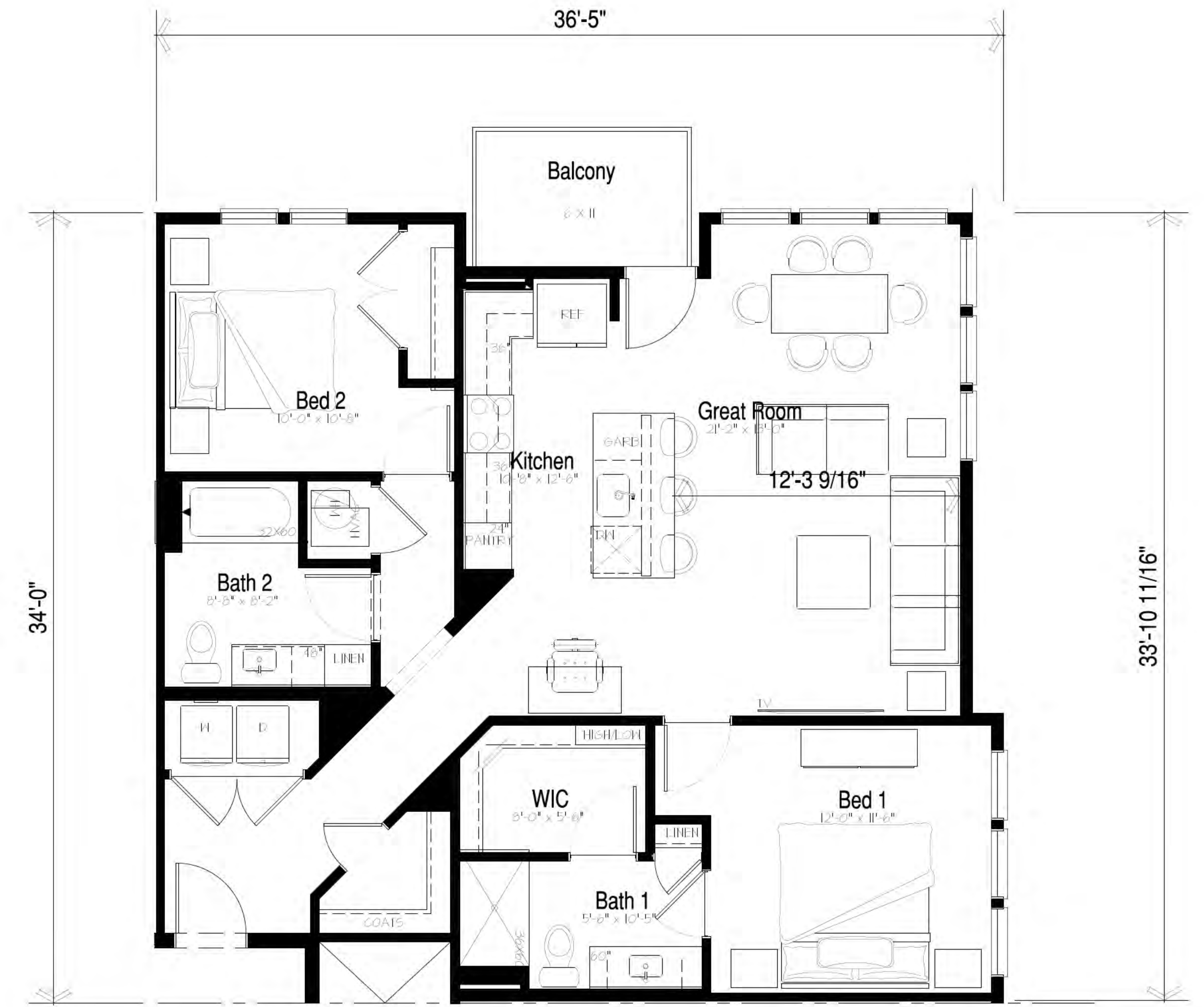
ANSI Type B: 1 Bedroom / 1 Bath  
 NRSF: 680  
**Unit - A1 Floor Plan**

SCALE: 1/4"=1'-0" A11-11



ANSI Type B: 2 Bedroom / 2 Bath  
 NRSF: 1035  
**Unit - B1 Floor Plan**

SCALE: 1/4"=1'-0" A11-11



ANSI Type B, 2 Bedroom / 2 Bath  
 1125 GSF  
**Unit - B2 2-Bedroom**

SCALE: 1/4"=1'-0" A11-11

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Conceptual Unit Plans

station  
 85 - hundred  
 8500 Lehigh Avenue  
 Morton Grove, Illinois

**BSB**  
 DESIGN  
 BSBDESIGN.COM

Revised August 5, 2025



DEVELOPMENT  
INVESTMENT  
CONSTRUCTION

### Exterior Elevations

**station 85 hundred**  
8500 Lehigh Avenue, Morton Grove, IL



August 11, 2025



**East (Primary) Elevation**  
Scale: 3/32" = 1'-0"

East Facade Transparency: 53.8%  
Overall Facade Transparency: 51.7%

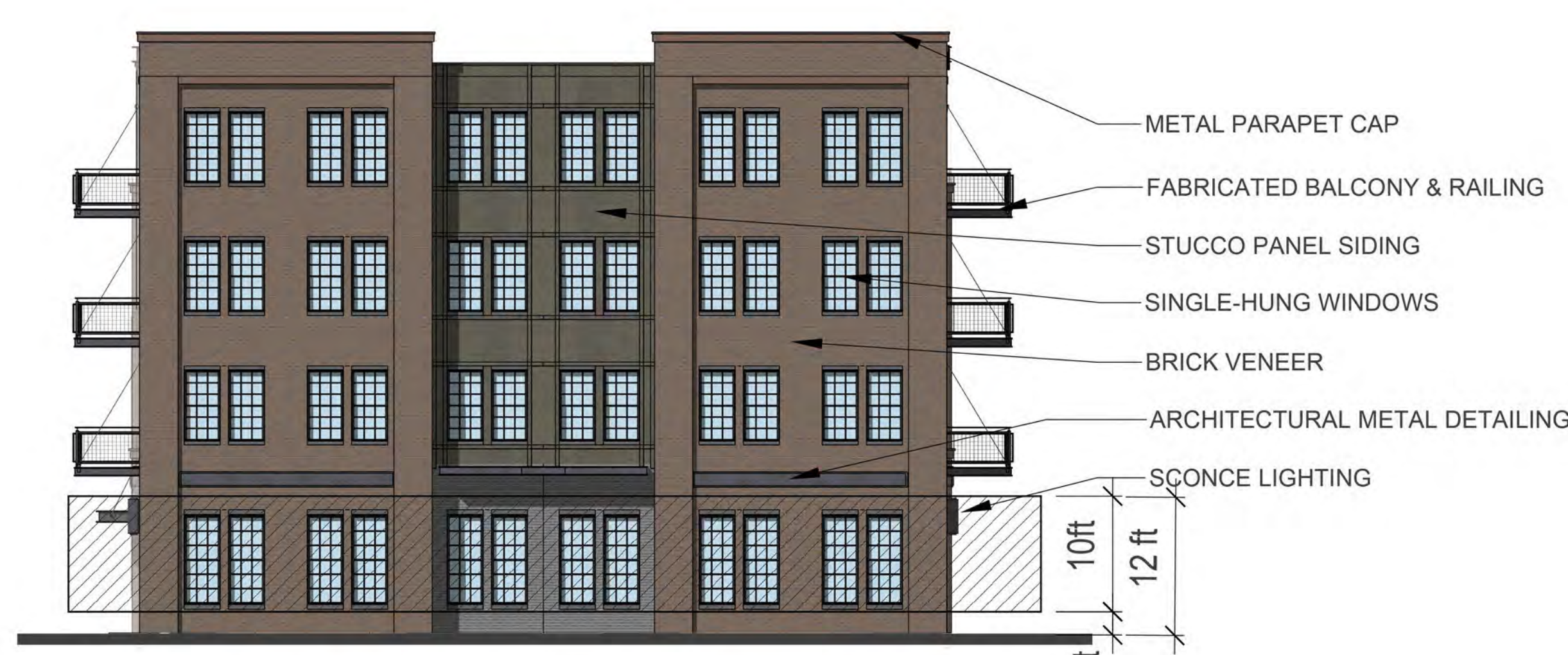


**West (Rear) Elevation**  
Scale: 3/32" = 1'-0"

West Facade Transparency: 46.7%  
Overall Facade Transparency: 51.7%



**North Elevation**  
Scale: 3/32" = 1'-0"  
North Facade Transparency: 71.9%  
Overall Facade Transparency: 51.7%



**South Elevation**  
Scale: 3/32" = 1'-0"  
South Facade Transparency: 46.8%  
Overall Facade Transparency: 51.7%

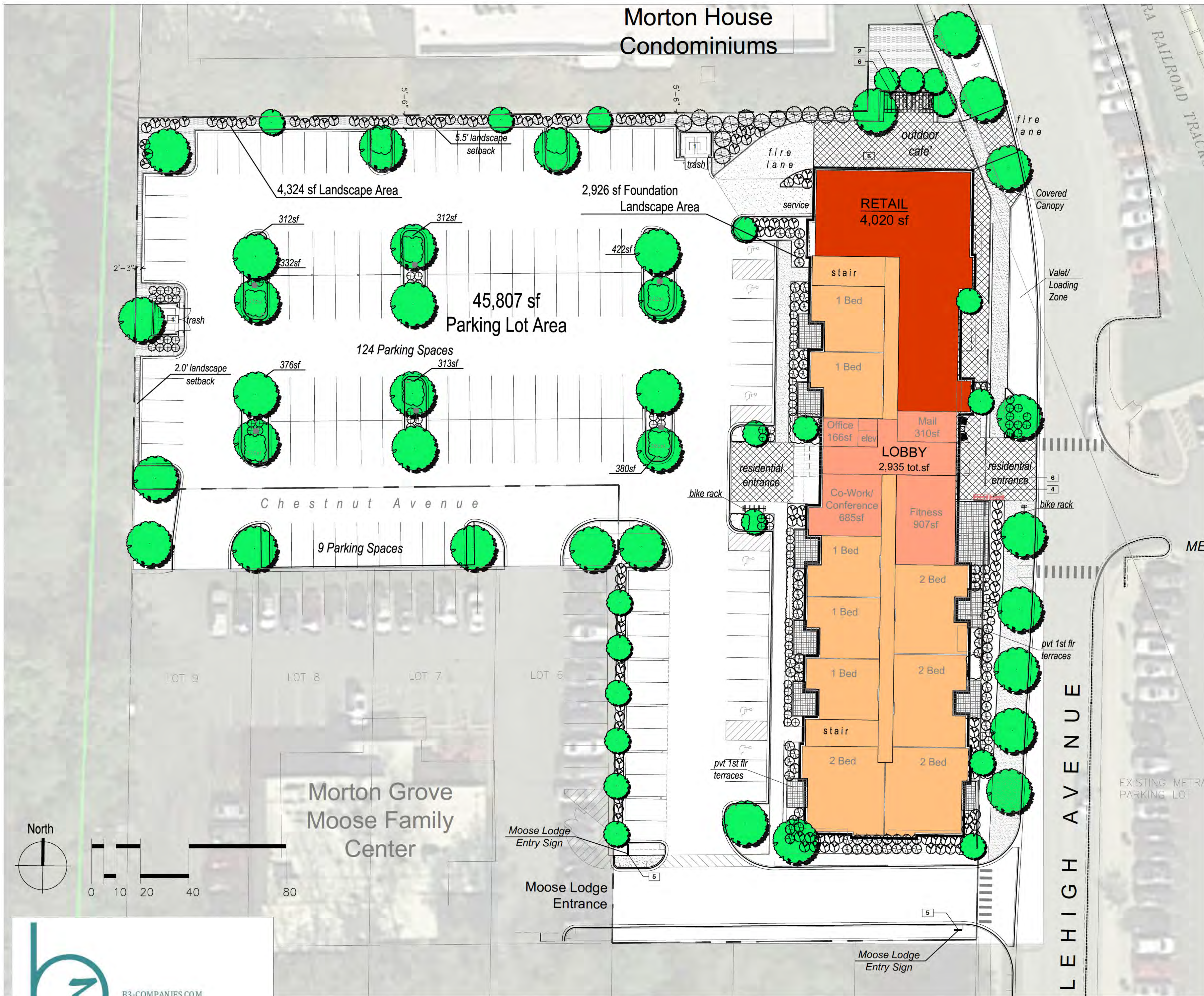
Exterior Materials Legend	
	Brick Veneer
	Belden Brandywine Velour
	Windows and Balcony Doors
	Anderson Fibrex or Equivalent
	Sconce Lighting
	Chara 12 Outdoor
	Aluminum Storefront
	Kawneer - Dark Bronze
	Fabricated Balconies & Metals
	Midwest Iron
	Stucco Panel Siding
	James Hardie
	Engineered Panel Siding
	James Hardie



**Exterior Elevations**

**station 85 hundred**  
8500 Lehigh Avenue, Morton Grove, IL





**Station 85-Hundred**  
**Mixed Use Residential/Retail:**  
 60 Total Units

32 - 1 bed	58%
3 - 1 bed+den	
25 - 2 bed	42%

2,935 sf Amenity  
 4,140 sf Retail  
 4,020 sf divisible retail  
 1,100 sf Cafe'

124 Parking  
 84 residential 1sp/bed  
 40 commercial 1 sp/1000sf  
 +9 Shared Guest

**Landscape Areas:**

Parking Lot Area	45,807sf
Interior Landscape Area	4,324sf
Foundation Lndscp Area	2,926sf



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Site Plan

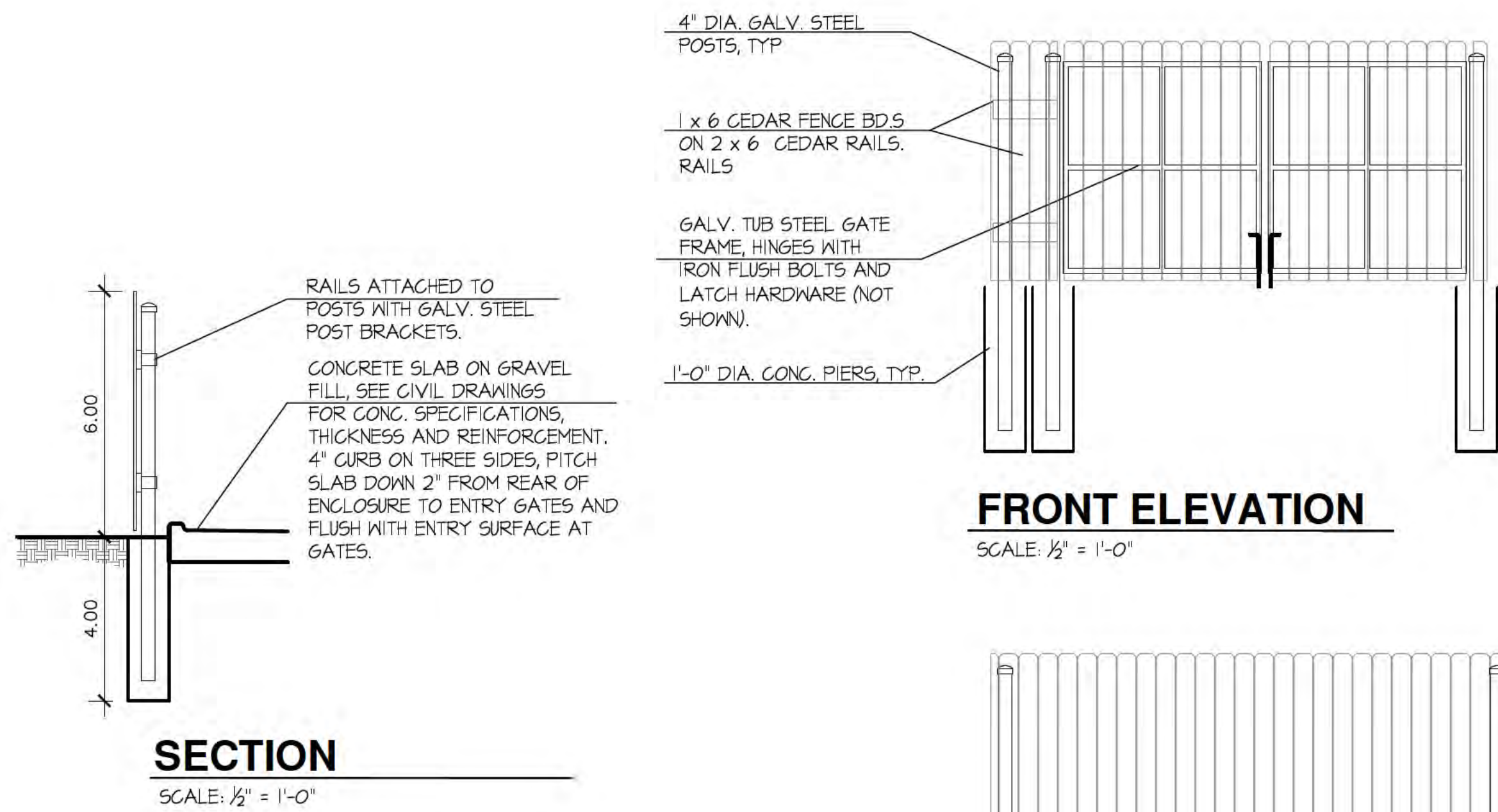
station  
 85 - hundred  
 8500 Lehigh Avenue  
 Morton Grove, Illinois



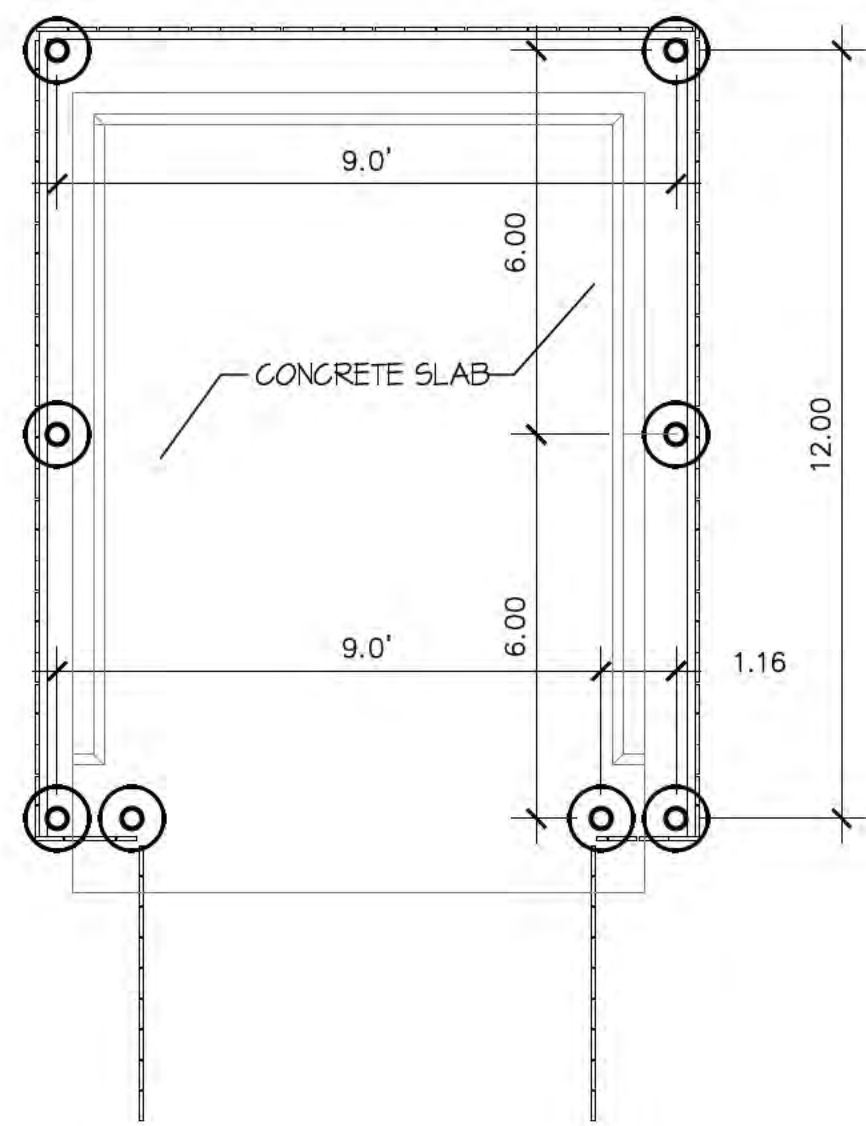
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Revised August 26, 2025

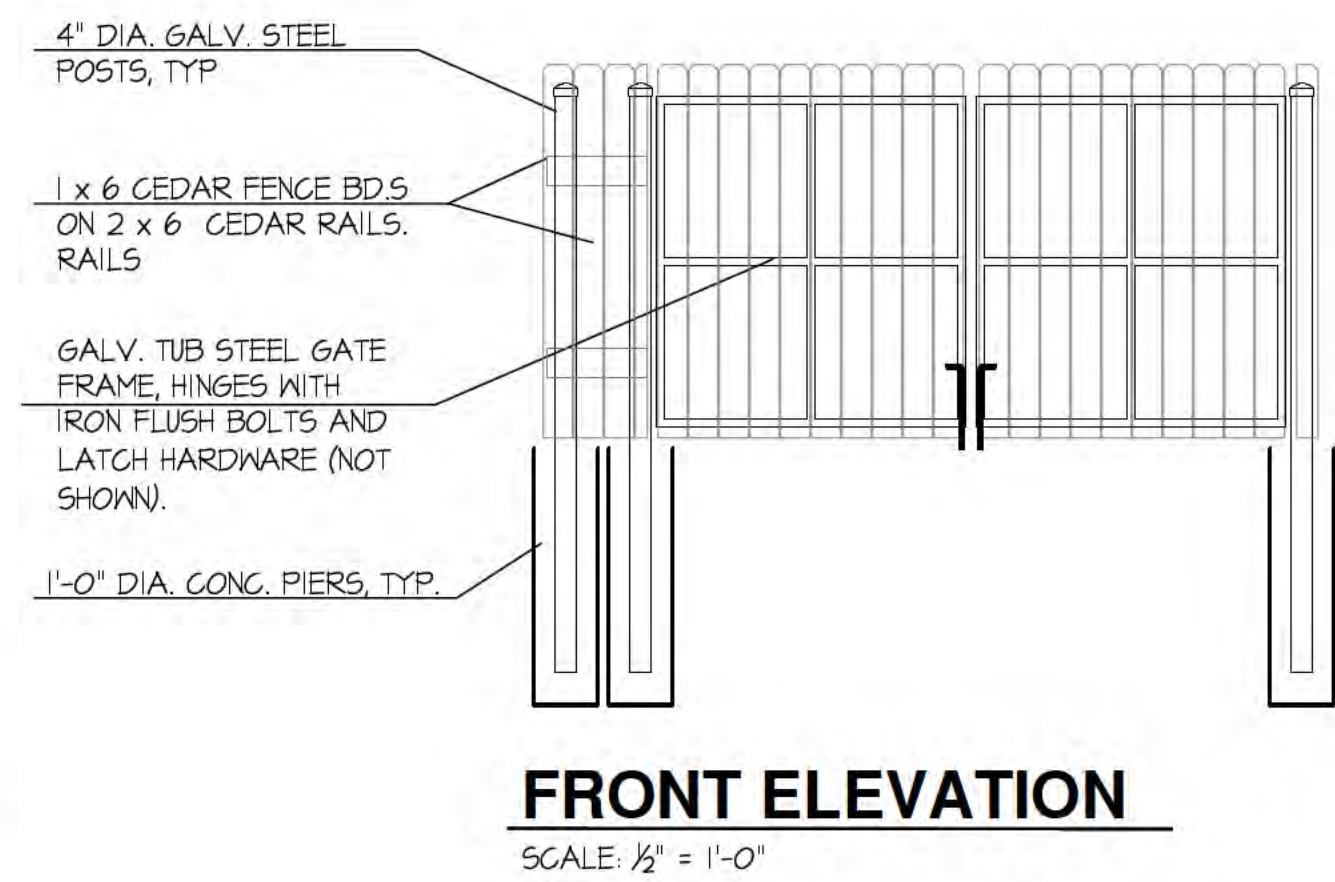




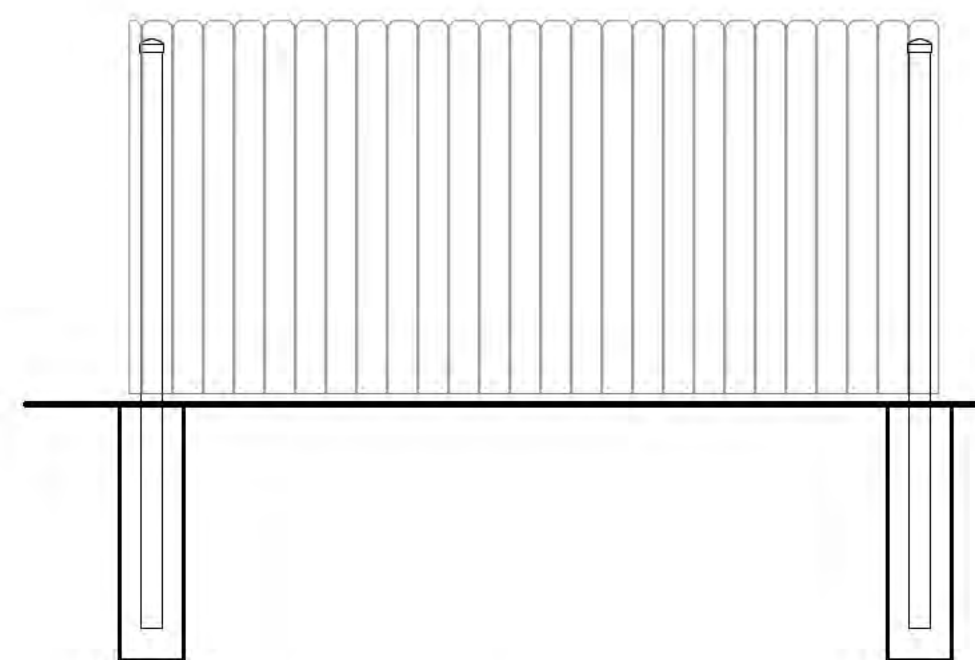
**SECTION**  
SCALE: 1/2" = 1'-0"



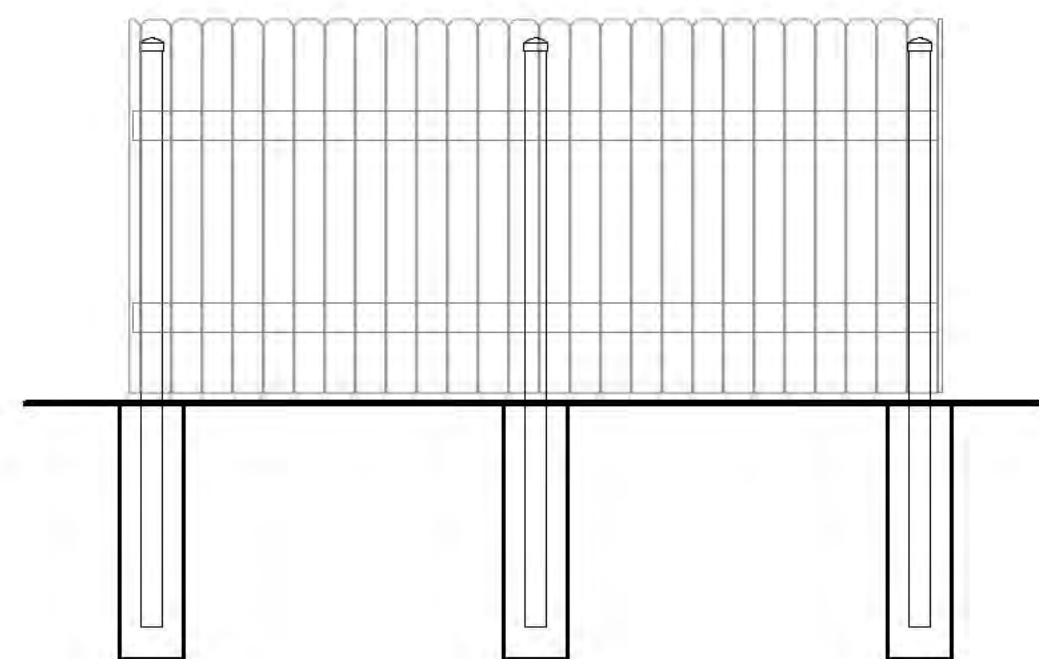
**1 TRASH ENCLOSURE PLAN**  
SCALE: 1/2" = 1'-0"



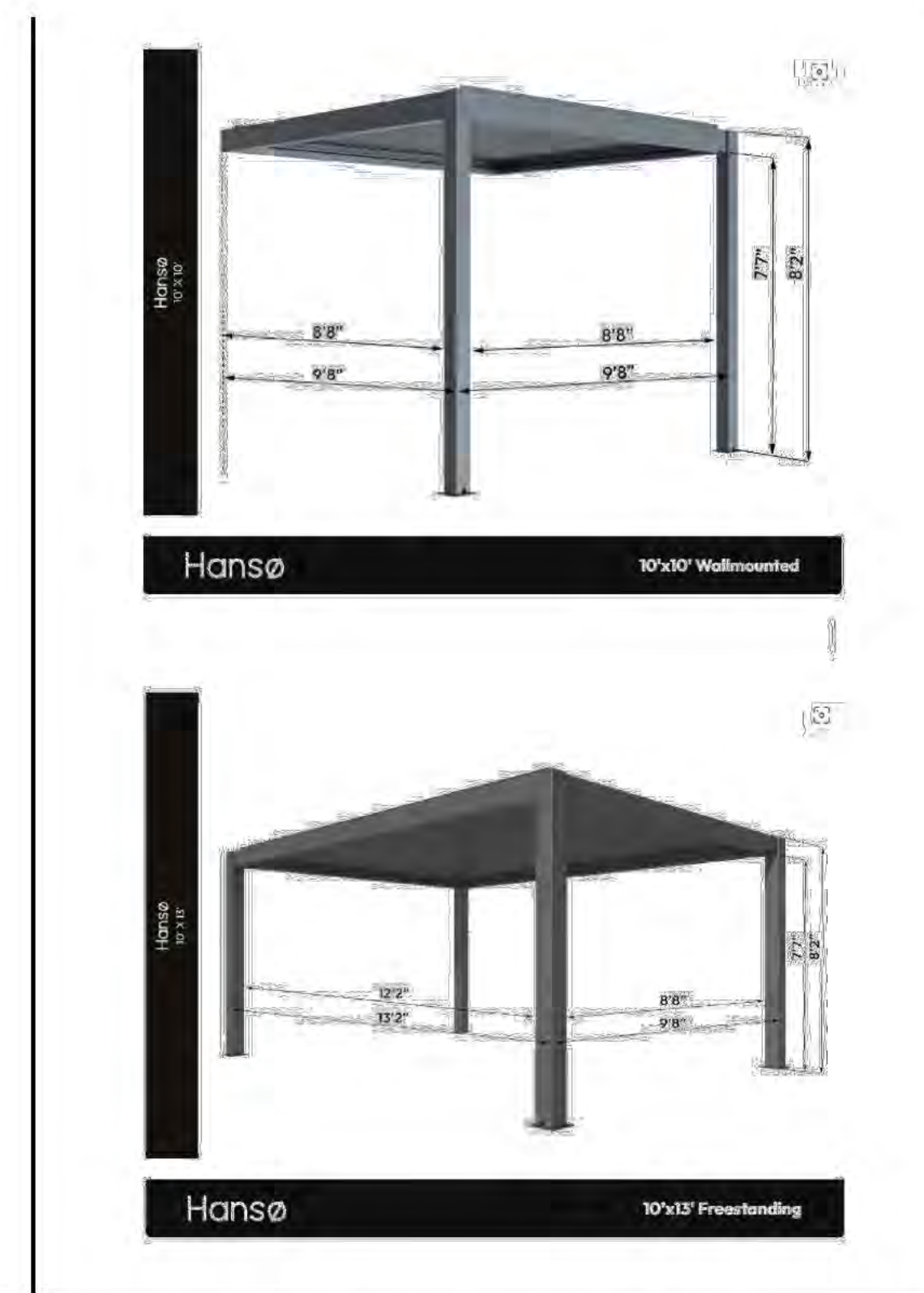
**FRONT ELEVATION**  
SCALE: 1/2" = 1'-0"



**REAR ELEVATION**  
SCALE: 1/2" = 1'-0"

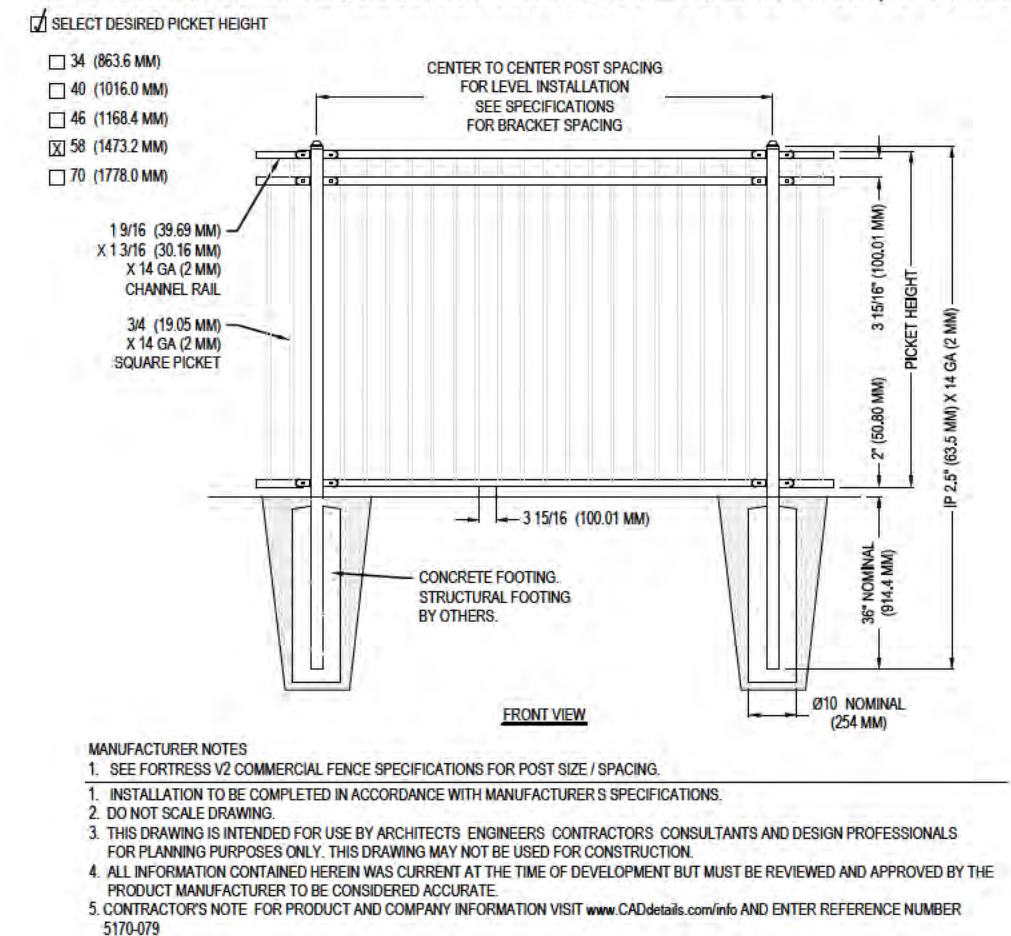


**SIDE ELEVATION**  
SCALE: 1/2" = 1'-0"

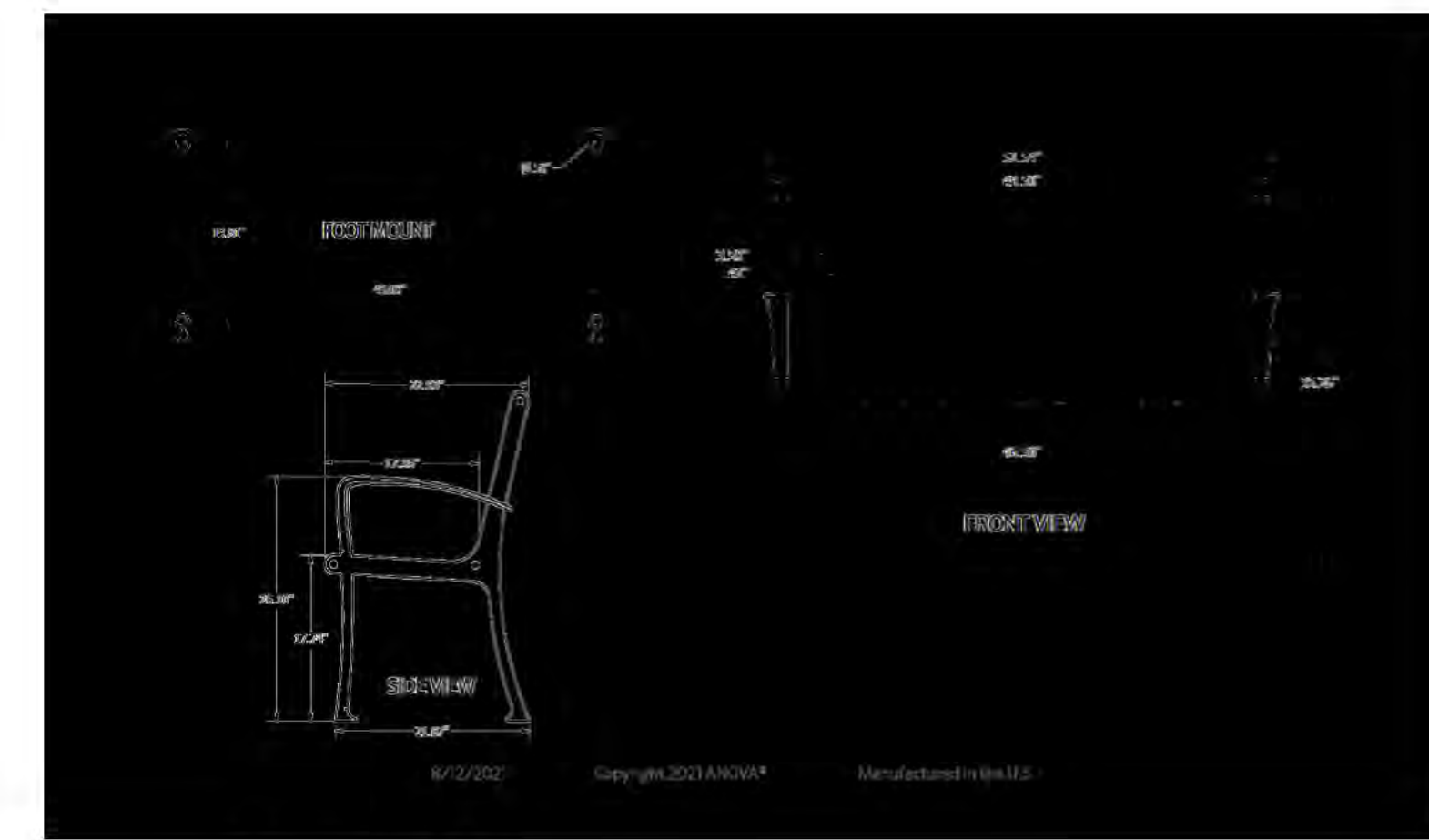


**2 PERGOLA - SHELTER**  
Hanso Structures

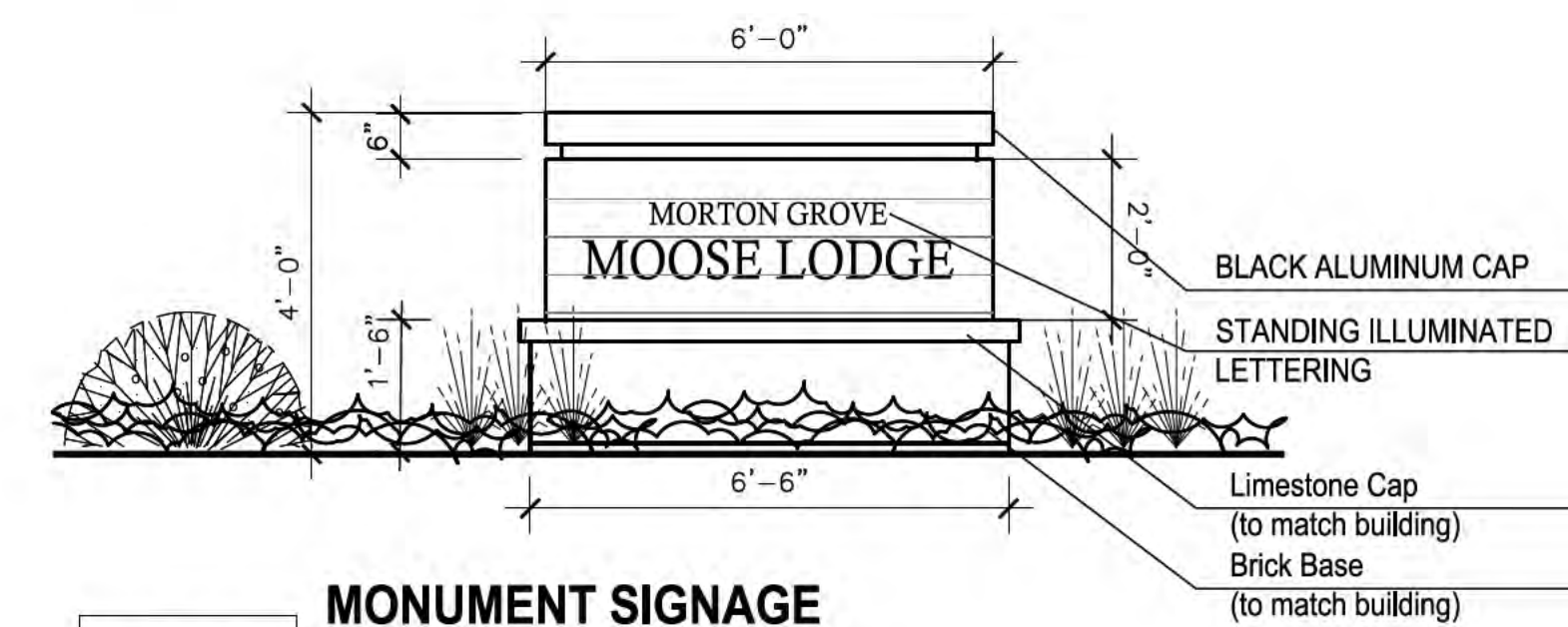
V2 COMMERCIAL ALUMINUM FENCE: FLAT TOP, 3 RAIL



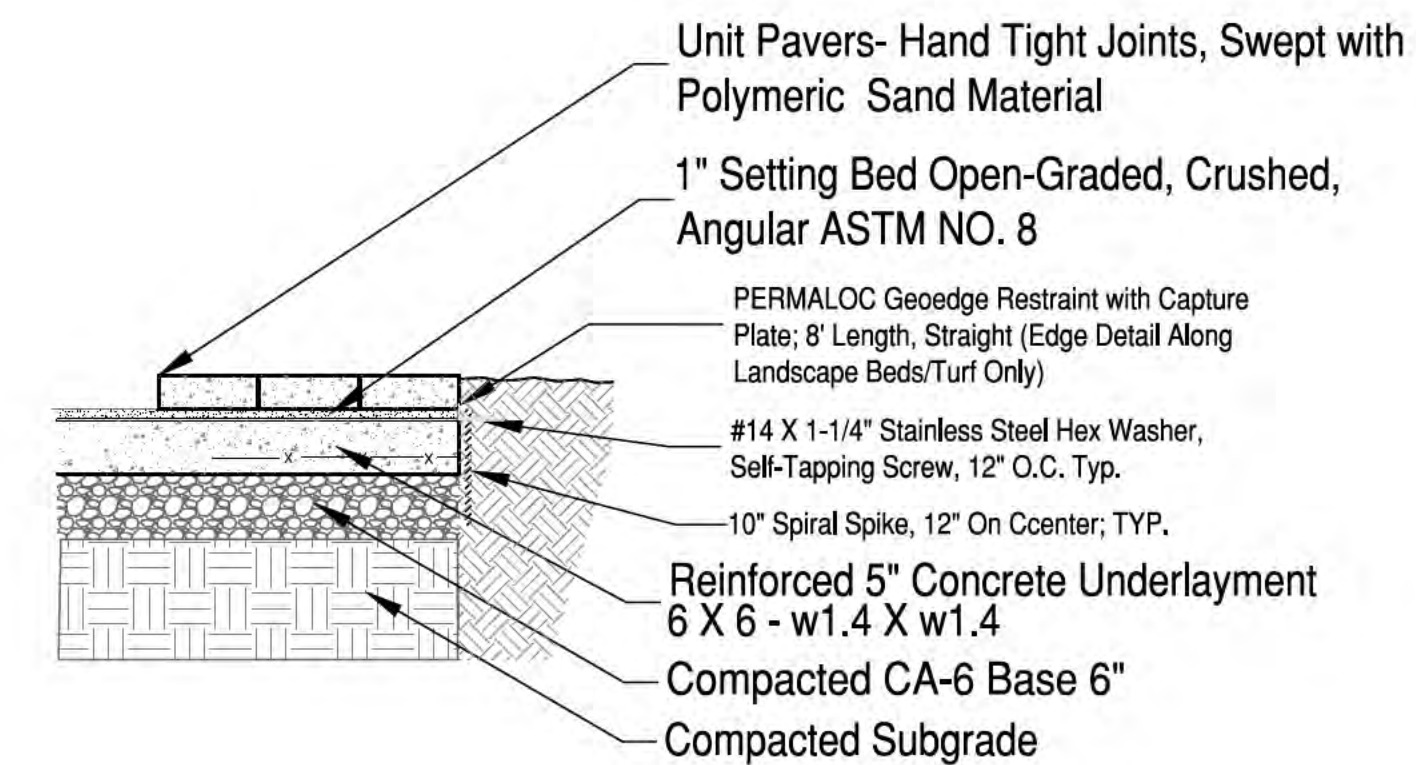
**3 ORNAMENTAL FENCE**



**4 BENCH**



**5 MONUMENT SIGNAGE SIGNAGE**



**6 SIDEWALK PAVER SECTION**



Conceptual Landscape Details

station  
85 - hundred  
8500 Lehigh Avenue  
Morton Grove, Illinois



COOK COUNTY FOREST PRESERVE

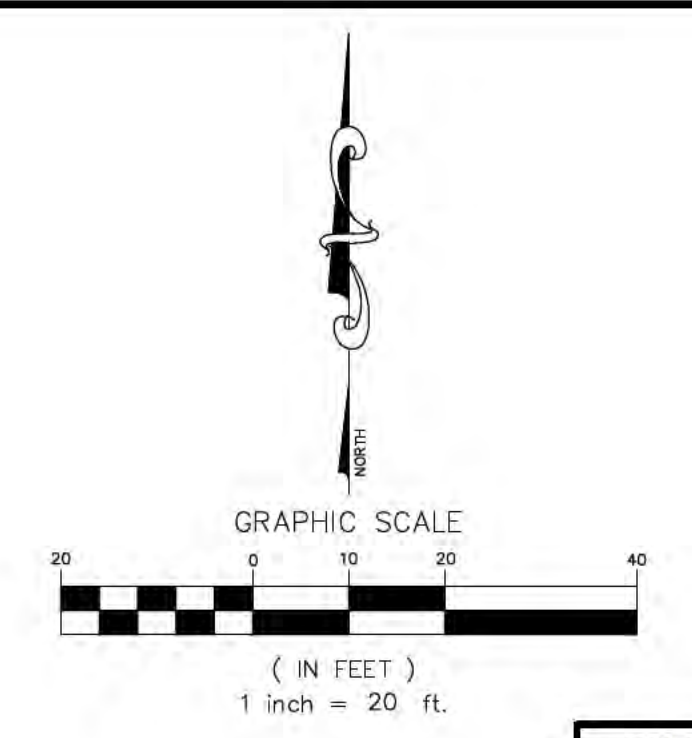
EXISTING BUILDING

RESIDENTIAL BUILDING

LEHIGH AVENUE  
66.00' R.O.M.

METRA RAILROAD TRACKS

LEHIGH AVENUE  
AS OCCUPIED



**ABBREVIATIONS LEGEND:**

EX	=	EXISTING
PR	=	PROPOSED
BC	=	BACK OF CURB
FC	=	FACE OF CURB
EP	=	EDGE OF PAVEMENT
PL	=	PROPERTY LINE
FB	=	FACE OF BUILDING
FW	=	FACE OF WALK (SIDEWALK)
ROW	=	RIGHT OF WAY
BC/BC	=	BACK OF CURB TO BACK OF CURB
SW	=	SIDEWALK
R	=	RADIUS
RW	=	RETAINING WALL
(TYP)	=	TYPICAL

**SITE DATA:**

TOTAL SITE SIZE	=	82,781 S.F. (1.90 AC)
EXISTING SITE CONDITIONS:		
EXISTING BUILDING FOOTPRINT	=	0 S.F. (0.0%)
EXISTING PAVT./SIDEWALK	=	44,532 S.F. (53.79%)
EXISTING GREENSPACE	=	38,249 S.F. (46.21%)
EXISTING CONDITION IMPERVIOUS AREA	=	44,532 S.F. (1.02 AC)
PROPOSED SITE CONDITIONS:		
PROPOSED BUILDING FOOTPRINTS	=	17,336 S.F. (20.94%)
PROPOSED PAVT./SIDEWALK	=	54,635 S.F. (66.00%)
PROPOSED GREENSPACE	=	10,810 S.F. (13.06%)
PROPOSED CONDITION IMPERVIOUS AREA	=	71,971 S.F. (1.65 AC)
PROPOSED PARKING SUMMARY:		
REGULAR STALLS (9'x18')	=	118
ADA PARKING STALLS (8'x18')	=	6
TOTAL STALLS PROVIDED	=	124

**STORM WATER STORAGE SUMMARY**

DETENTION STORAGE REQUIRED=	0.607 AC-FT
DETENTION STORAGE PROVIDED=	0.612 AC-FT
VOLUME CONTROL STORAGE REQUIRED=	0.138 AC-FT
VOLUME CONTROL STORAGE PROVIDED=	0.1402 AC-FT
VOLUME CONTROL CONTECH SYSTEM=	0.1402 AC-FT
DETENTION CONTECH SYSTEM=	0.612 AC-FT
VC REQUIRED =	(1.66AC)*(1')*(1')/(12") = 0.138 AC-FT
SEASONAL HIGH GROUND WATER LEVEL=	(±) 10' DEEP
APPLIED GEO SCIENCE INC. SOIL BORINGS DATED	FEBRUARY 2024.

**SURFACE IMPROVEMENT LEGEND:**

	EXISTING SIDEWALK
	EXISTING ASPHALT PAVEMENT
	NEW ASPHALT PAVEMENT 1.5" - HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 BITUMINOUS TACK COAT (0.05 GAL/SY) 2.5" HOT-MIX ASPHALT BINDER COURSE, IL-19, N50 BITUMINOUS PRIME COAT MC-30 (0.30 GAL/SY) 8" AGGREGATE BASE COURSE CA-6, TYPE B
	NEW ASPHALT PVM. (HEAVY DUTY) 1.5" - HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 BITUMINOUS TACK COAT 3" HOT-MIX ASPHALT BINDER COURSE, IL-19, N50 BITUMINOUS PRIME COAT MC-30 12" AGGREGATE BASE COURSE CA-6, TYPE B
	CONCRETE SIDEWALKS 4" - PCC SIDEWALK (4,000 PSI)(5" PUBLIC SIDEWALKS) 3" - AGGREGATE BASE COURSE TYPE B, CA-6 (4" PUBLIC SIDEWALKS)
	PROPOSED B6.12 CONCRETE CURB AND GUTTER
	PROPOSED REVERSE PITCH B6.12 CURB AND GUTTER
	PROPOSED DEPRESSED CURB AND GUTTER
	PROPOSED MOUNTABLE CURB AND GUTTER
	EXISTING CURB AND GUTTER
	EXISTING DEPRESSED CURB AND GUTTER

- SITE GEOMETRIC AND PAVING NOTES:**
- SIDEWALK RAMPS WITH DETECTABLE WARNINGS AND DEPRESSED CURBS SHALL BE INSTALLED AT ALL SIDEWALK CROSSINGS. SEE CONSTRUCTION STANDARDS FOR SPECIFIC DETAILS.
  - UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO THE BACK OF CURB, FACE OF BUILDING, OR PROPERTY LINES.
  - UNLESS OTHERWISE NOTED, ALL CURB AND GUTTER SHALL BE B6.12 CONCRETE CURB AND GUTTER. (SEE CONSTRUCTION STANDARDS FOR SPECIFIC DETAILS.)
  - UNLESS OTHERWISE NOTED, ALL CURB RADI ARE 4' TO BACK OF CURB.
  - ALL BOUNDARY AND LOT DIMENSIONS ARE SHOWN PER THE SUBDIVISION (OR SITE) PLAT PREPARED BY TERRA TECHNOLOGY LAND SURVEYING, INC. AND DATED DECEMBER 28, 2021.
  - BUILDING DIMENSIONS HAVE BEEN INDICATED HEREON BASED UPON ARCHITECTURAL INFORMATION CURRENT AS OF THE BASE DATE OF THIS PLAN PREPARATION. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR PRECISE BUILDING DIMENSIONS AND ADVISE THE ARCHITECT AND ENGINEER OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION.
  - IMPROVEMENTS ADJACENT TO BUILDINGS, IF SHOWN (SUCH AS TRUCK DOCKS, RETAINING WALLS, SIDEWALKS, CURBING, FENCING, CANOPIES, RAMPS, HANDICAP ACCESS, PLANTERS, DUMPSTERS, TRANSFORMERS, BOLLARDS, ETC) HAVE BEEN SHOWN FOR APPROXIMATE LOCATION ONLY. REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS.
  - THE LOCATION OF PRIVATE SIDEWALKS SHALL BE COORDINATED WITH PROPOSED DOORWAYS. CONTRACTOR TO VERIFY ACTUAL DOORWAY LOCATION WITH ARCHITECTURAL PLANS PRIOR TO CONSTRUCTING SIDEWALKS.
  - ALL STRUCTURAL AND ARCHITECTURAL DESIGN DATA FOR THE MASONRY WALLED TRASH ENCLOSURES ARE THE RESPONSIBILITY OF THE PROJECT ARCHITECT. REFER TO THE ARCHITECTURAL PLANS FOR ALL DETAILS PERTAINING TO SAME.
  - UPON COMPLETION OF PAVING OPERATIONS, THE CONTRACTOR SHALL INSTALL THE PAVEMENT MARKINGS AND STRIPES AND ALL DIRECTIONAL SIGNAGE, ETC AS SHOWN HEREON. PARKING STALL (EXCEPT FOR HC) MARKING COLOR IS WHITE. ALL ON-SITE PAVEMENT MARKINGS AND STRIPES SHALL BE PAINTED WITH IDOT SPECIFICATION PAVEMENT PAINT. PARKING STALL STRIPES SHALL BE 4" WIDE. HANDICAP STALLS SHALL BE PAINTED YELLOW AND SIGNED PER FEDERAL, STATE AND LOCAL REQUIREMENTS.
  - PRIOR TO OPENING TO THE PUBLIC, ALL TRAFFIC CONTROL SIGNAGE SHALL BE INSTALLED AS INDICATED. SIGNS SHALL BE INSTALL WITH 3" SQUARE ALUMINUM POSTS WITH A BAKED ON ENAMEL FINISH, SET 1' INTO CONCRETE PIER AND SHALL INCLUDE A POST CAP.

DRAWN BY: \_\_\_\_\_ DATE: \_\_\_\_\_

MORTON GROVE RESIDENTIAL  
MORTON GROVE, ILLINOIS  
PRELIMINARY SITE GEOMETRIC AND PAVING PLAN

975 E. 22nd St, Suite 400  
Wheaton, IL 60189  
630.480.7899  
www.rwg-engineering.com

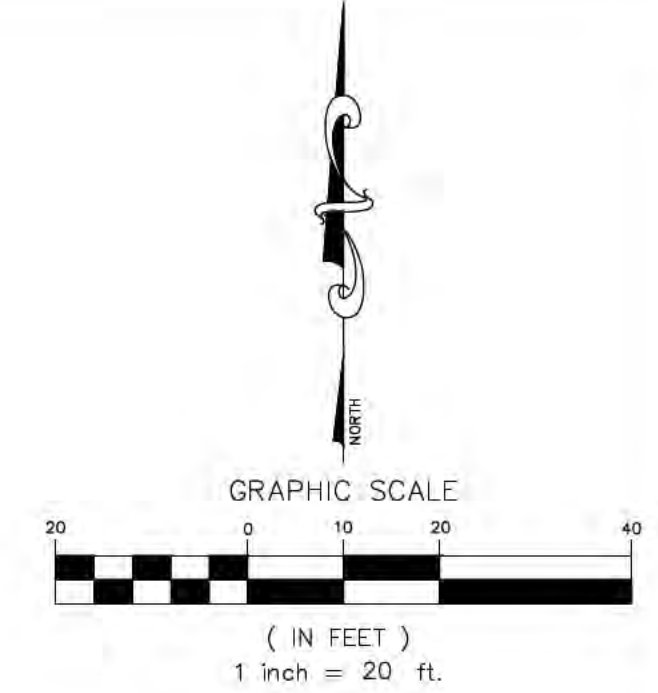
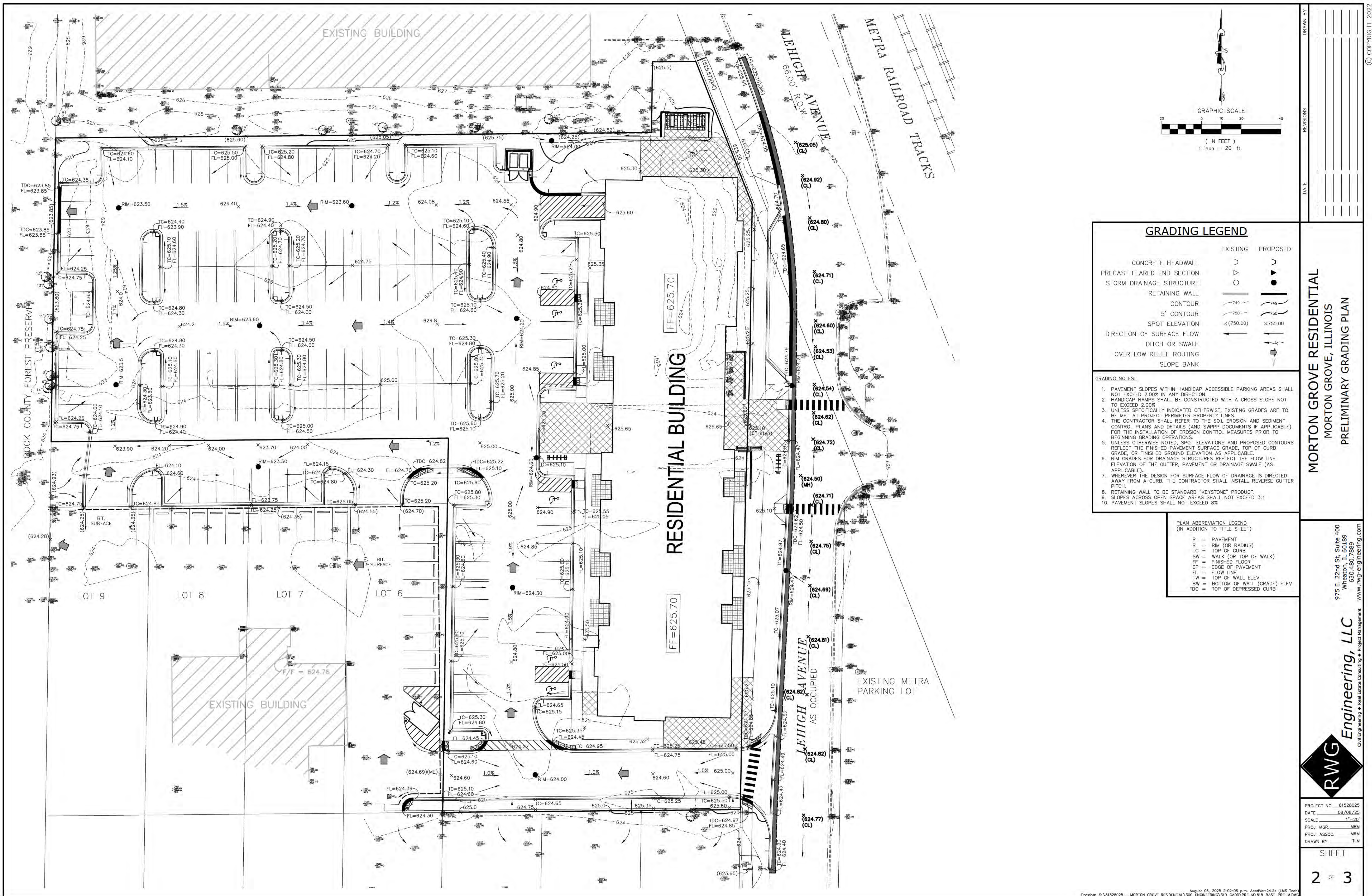


PROJECT NO. 81528025  
DATE 08/08/25  
SCALE 1"=20'  
PROJ. MGR. MRM  
PROJ. ASSOC. MRM  
DRAWN BY TLM

SHEET

1 OF 3

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**GRADING LEGEND**

	EXISTING	PROPOSED
CONCRETE HEADWALL		
PRECAST FLARED END SECTION		
STORM DRAINAGE STRUCTURE		
RETAINING WALL		
CONTOUR		
5' CONTOUR		
SPOT ELEVATION		
DIRECTION OF SURFACE FLOW		
DITCH OR SWALE		
OVERFLOW RELIEF ROUTING		
SLOPE BANK		

- GRADING NOTES:**
- PAVEMENT SLOPES WITHIN HANDICAP ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2.00% IN ANY DIRECTION.
  - HANDICAP RAMPS SHALL BE CONSTRUCTED WITH A CROSS SLOPE NOT TO EXCEED 2.00%.
  - UNLESS SPECIFICALLY INDICATED OTHERWISE, EXISTING GRADES ARE TO BE MET AT PROJECT PERIMETER PROPERTY LINES.
  - THE CONTRACTOR SHALL REFER TO THE SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS (AND SWPPP DOCUMENTS IF APPLICABLE) FOR THE INSTALLATION OF EROSION CONTROL MEASURES PRIOR TO BEGINNING GRADING OPERATIONS.
  - UNLESS OTHERWISE NOTED, SPOT ELEVATIONS AND PROPOSED CONTOURS REFLECT THE FINISHED PAVEMENT SURFACE GRADE, TOP OF CURB GRADE, OR FINISHED GROUND ELEVATION AS APPLICABLE.
  - RIM GRADES FOR DRAINAGE STRUCTURES REFLECT THE FLOW LINE ELEVATION OF THE GUTTER, PAVEMENT OR DRAINAGE SWALE (AS APPLICABLE).
  - WHEREVER THE DESIGN FOR SURFACE FLOW OF DRAINAGE IS DIRECTED AWAY FROM A CURB, THE CONTRACTOR SHALL INSTALL REVERSE GUTTER FITCH.
  - RETAINING WALL TO BE STANDARD "KEYSTONE" PRODUCT.
  - SLOPES ACROSS OPEN SPACE AREAS SHALL NOT EXCEED 3:1.
  - PAVEMENT SLOPES SHALL NOT EXCEED 8%.

**PLAN ABBREVIATION LEGEND**  
(IN ADDITION TO TITLE SHEET)

P	=	PAVEMENT
R	=	RIM (OR RADIUS)
TC	=	TOP OF CURB
SW	=	WALK (OR TOP OF WALK)
FF	=	FINISHED FLOOR
EP	=	EDGE OF PAVEMENT
FL	=	FLOW LINE
TW	=	TOP OF WALL ELEV
BW	=	BOTTOM OF WALL (GRADE) ELEV
TDC	=	TOP OF DEPRESSED CURB

REVISIONS

NO.	DATE	DESCRIPTION

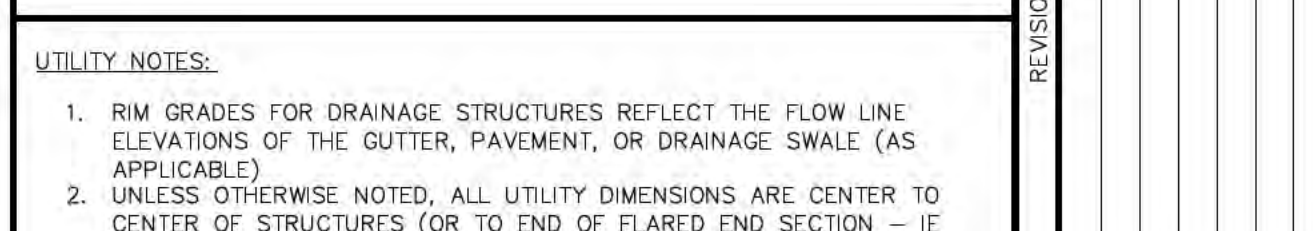
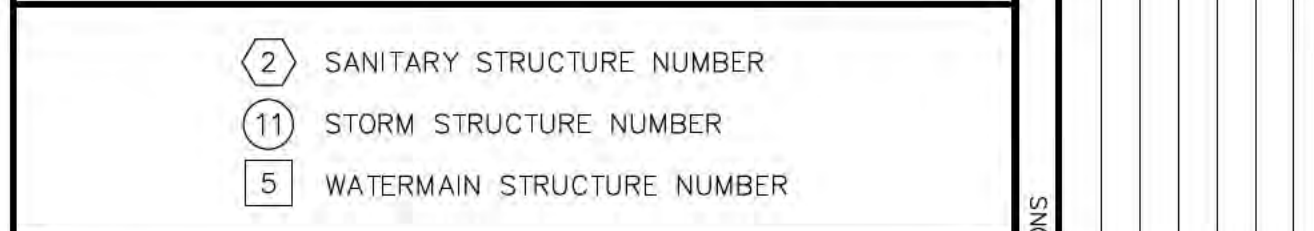
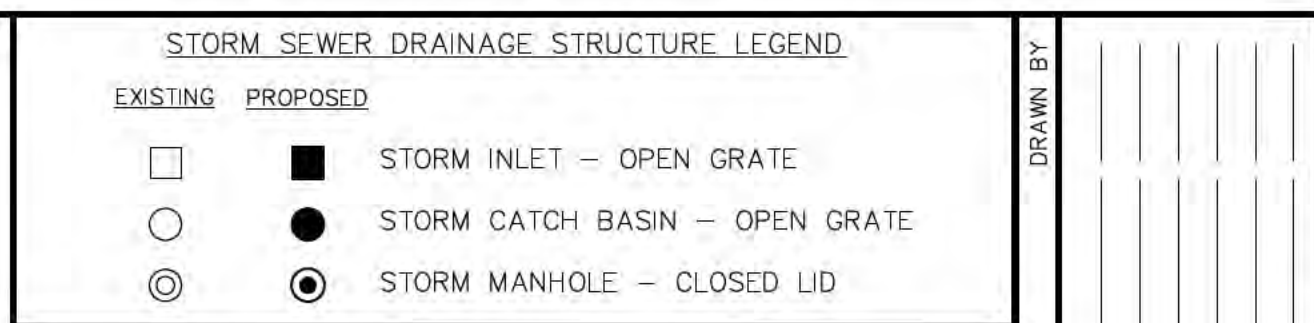
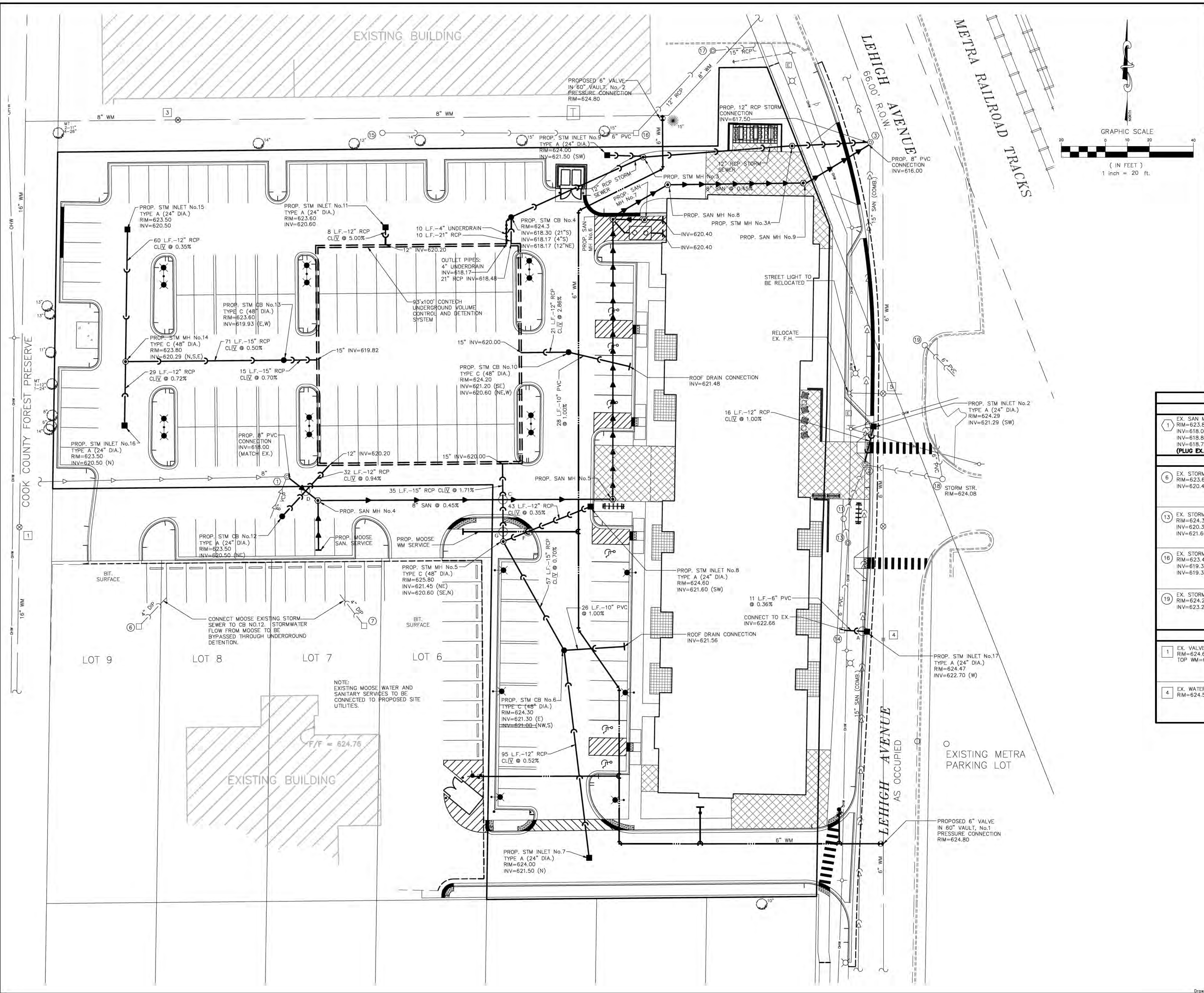
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PROJECT NO. 181528025  
DATE: 08/08/25  
SCALE: 1"=20'  
PROJ. MOR: MSM  
PROJ. ASSOC: MSM  
DRAWN BY: TLM



STORM SEWER DRAINAGE STRUCTURE LEGEND

EXISTING PROPOSED

○ □ STORM INLET - OPEN GRATE

● ○ STORM CATCH BASIN - OPEN GRATE

⊙ ○ STORM MANHOLE - CLOSED LID

② SANITARY STRUCTURE NUMBER

①① STORM STRUCTURE NUMBER

⑤ WATERMAIN STRUCTURE NUMBER

UTILITY NOTES:

1. RIM GRADES FOR DRAINAGE STRUCTURES REFLECT THE FLOW LINE ELEVATIONS OF THE GUTTER, PAVEMENT, OR DRAINAGE SWALE (AS APPLICABLE).
2. UNLESS OTHERWISE NOTED, ALL UTILITY DIMENSIONS ARE CENTER TO CENTER OF STRUCTURES (OR TO END OF FLARED END SECTION - IE INCLUDING LENGTH OF FLARED END SECTION).
3. THE CONTRACTOR SHALL ADJUST RIM ELEVATIONS OF ALL EXISTING STRUCTURES TO THE PROPOSED GRADES AS INDICATED ON THE PLANS.
4. CONNECTIONS TO EXISTING SEWERS OR WATERMANS (OR EXISTING SERVICE STUBS) AT POINTS OTHER THAN VISIBLE STRUCTURES ARE APPROXIMATE. THE CONTRACTOR SHALL EXCAVATE AND VERIFY EXISTING SEWER OR WATERMAIN LOCATIONS, SIZES, ELEVATIONS, AND PIPE CONDITIONS AT PROPOSED CONNECTION POINTS PRIOR TO CONSTRUCTING UTILITY EXTENSIONS AND NOTIFY THE ENGINEER AND OWNER OF ANY CONFLICT OR DISCREPANCIES.
5. EXISTING UNDERGROUND PIPE, CONDUIT AND/OR CABLES (LIGHTING, ELECTRIC, GAS, CABLE, ETC) ARE SHOWN FROM RECORD INFORMATION AND ARE APPROXIMATE IN NATURE. THE CONTRACTOR SHALL VERIFY EXACT LOCATION IN THE FIELD AND NOTIFY THE ENGINEER AND OWNER OF ANY CONFLICT.
6. SELECT GRANULAR TRENCH BACKFILL IS REQUIRED FOR ALL UTILITY TRENCHES UNDER EXISTING OR PROPOSED PAVEMENT, DRIVEWAYS, PARKING LOTS, AND SIDEWALKS, AND EXTENDED A MINIMUM OF 2' EACH SIDE OF SAME GRANULAR TRENCH BACKFILL SHALL BE COMPACTED IN PLACE IN ACCORDANCE WITH THE SPECIFICATIONS.
7. BUILDING DIMENSIONS AND ADJACENT UTILITY SERVICE LOCATIONS HAVE BEEN PREPARED BASED UPON ARCHITECTURAL INFORMATION CURRENT AT THE TIME OF DRAWING PREPARATION. SUBSEQUENT ARCHITECTURAL CHANGES MAY EXIST. THE CONTRACTOR SHALL REFER TO THE CURRENT ARCHITECTURAL PLANS FIRST, FOR PRECISE BUILDING DIMENSIONS AND UTILITY SERVICE CONNECTION LOCATIONS AND NOTIFY THE ENGINEER AND ARCHITECT OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION.
8. ROUTING OF GAS, ELECTRIC, TELEPHONE AND OTHER CABLE SERVICES (IF SHOWN) ARE APPROXIMATE AND SUBJECT TO MODIFICATION BY THE RESPECTIVE UTILITY COMPANY AND/OR DEVELOPER. THE CONTRACTOR SHALL COORDINATE THE FINAL UTILITY SERVICE LOCATION WITH EACH UTILITY COMPANY PRIOR TO CONSTRUCTION.

EXISTING UTILITY SCHEDULE

SANITARY SEWER					
1	EX. SAN. M.H. RIM=623.50 (A) (24" DIA.) INV=618.00 (8" E.W.) INV=618.85 (6" VCP SW) INV=618.75 (6" VCP NE) <b>(PLUG EX. NE INVERT)</b>	2	EX. SAN. M.H. RIM=624.31 (A) (24" DIA.) INV=614.61 (N.S.) INV=617.96 (12" RCP NW) INV=618.86 (E) INV=619.21 (SW) INV=616.16 (W)	3	EX. SAN. M.H. RIM=625.08 INV=614.28 (15" NW,S)
STORM SEWER					
6	EX. STORM INLET RIM=623.68 INV=620.43 (4" D.I. NE)	7	EX. STORM INLET RIM=623.52 INV=620.00 (4" D.I. NW)	11	EX. STORM C.B. RIM=623.76 (A) (RIM=625.00) WATER=620.26 (NO VISIBLE PIPES) <b>(RIM TO BE CLOSED LID)</b>
13	EX. STORM M.H. RIM=624.30 (A) (24" DIA.) INV=620.30 (8" NE) INV=621.66 (6" PVC S)	14	EX. STORM INLET RIM=624.26 (A) (RIM=625.40) INV=622.66 (6" PVC N)	15	EX. STORM STRUCTURE RIM=624.07 (FULL OF SILT)
16	EX. STORM INLET RIM=623.44 INV=619.34 (12" RCP NE) INV=619.34 (6" PVC W)	17	EX. STORM M.H. RIM=626.28 INV=618.36 (15" RCP E) INV=618.38 (12" RCP SW)	18	EX. STORM M.H. RIM=624.08 INV=621.03 (6" PVC NW) INV=619.34 (6" PVC NW) <b>(COULD NOT OPEN)</b>
19	EX. STORM INLET RIM=624.24 INV=623.29 (6" PVC SE)	WATERMAIN			
1	EX. VALVE VAULT RIM=624.68 TOP WM=619.4	2	EX. VALVE VAULT (PROBABLE) RIM=623.60 TOP WM=618.4	3	EX. VALVE VAULT RIM=625.00 TOP WM=619.2
4	EX. WATER VALVE RIM=624.54	5	EX. VALVE VAULT RIM=624.17 TOP WM=620.0 FULL OF WATER		

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**RWG Engineering, LLC**  
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DATE 08/08/25  
SCALE 1"=20'  
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PROJ. ASSOC. MRM  
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SHEET 3 OF 3

August 06, 2025 2:03:31 p.m. AcqVer 24.2a (LMS Tech)  
Drawing: S:\81528025 - MORTON GROVE RESIDENTIAL\00\_ENGINEERING\3D CAD\DWG\BASE\_PRELIM.DWG

DRAWN BY: \_\_\_\_\_  
REVISIONS: \_\_\_\_\_  
DATE: \_\_\_\_\_

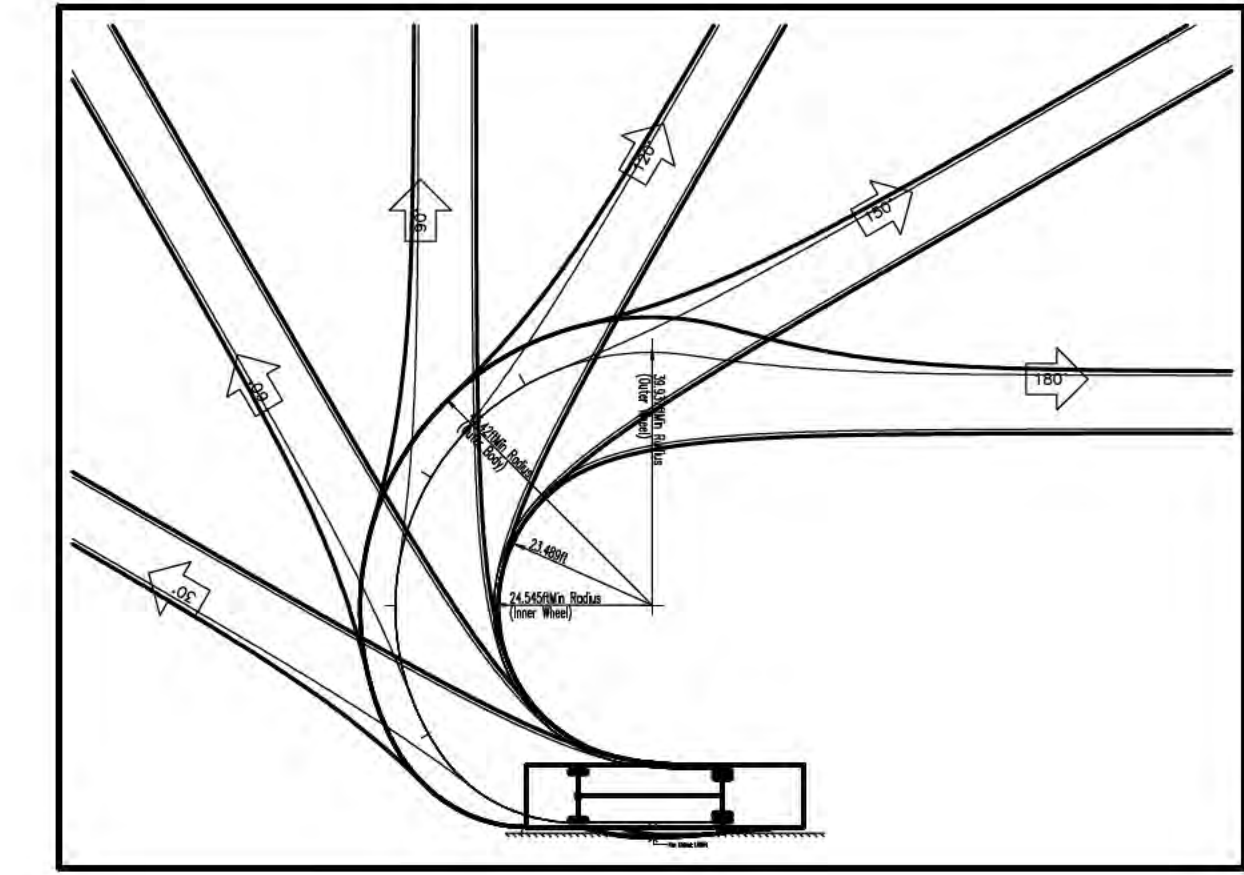
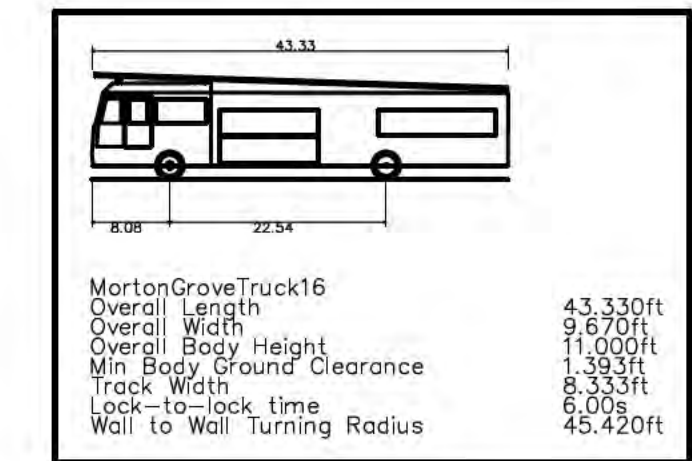
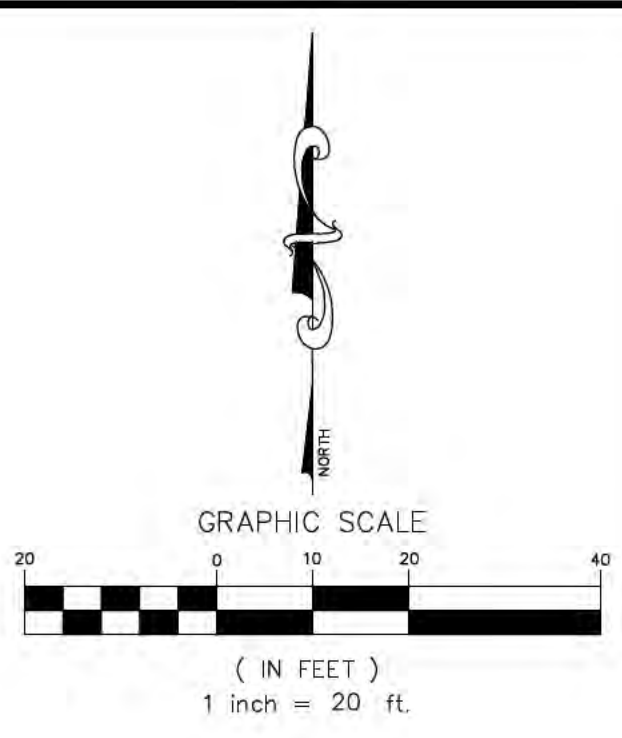
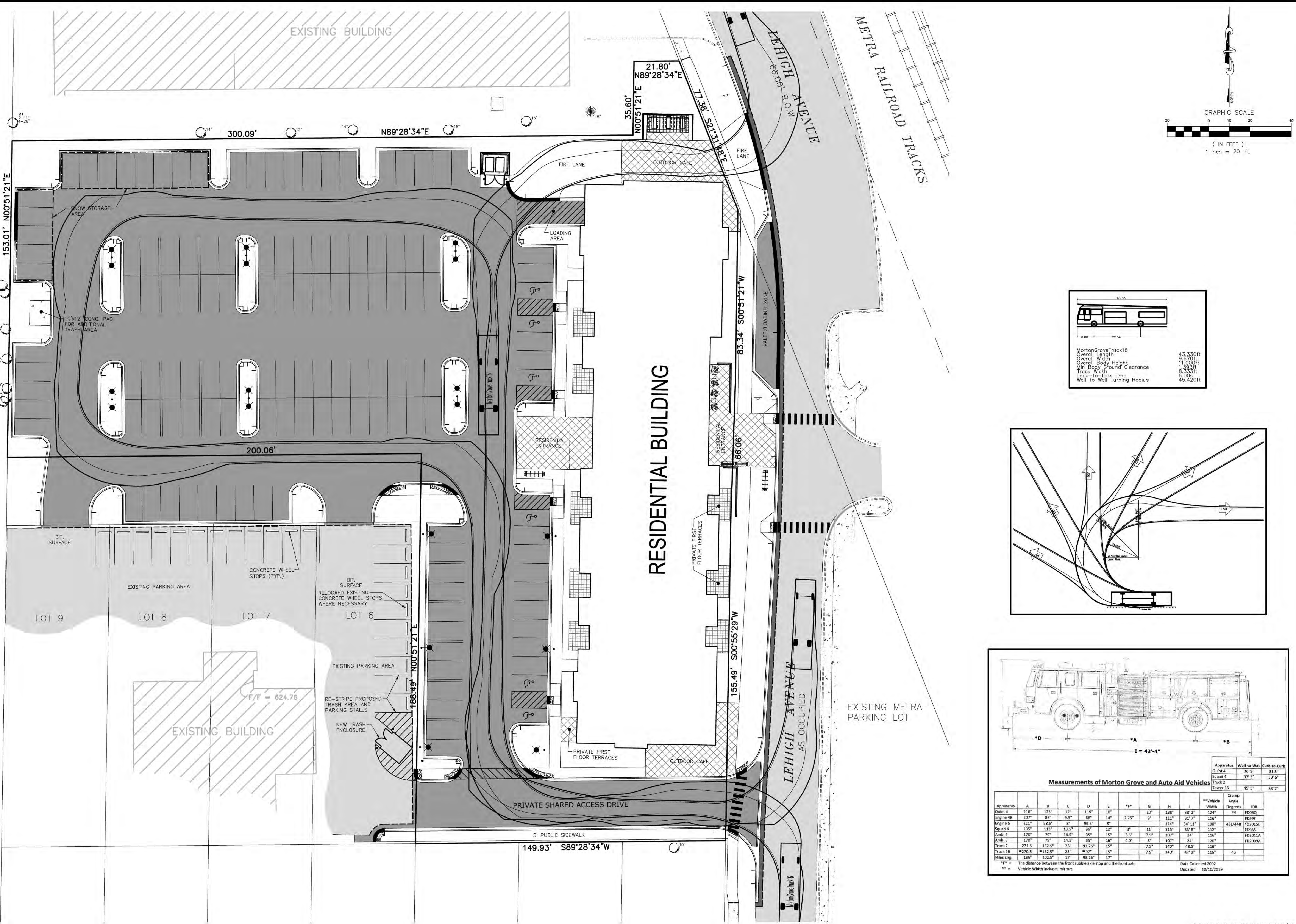
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COOK COUNTY FOREST PRESERVE

EXISTING BUILDING

# RESIDENTIAL BUILDING



Measurements of Morton Grove and Auto Aid Vehicles

Apparatus	A	B	C	D	E	**F	G	H	I	**Vehicle Angle Degrees	Cramp Angle Degrees	IDP
Quint 4	216"	123"	12"	119"	10"	10"	138"	38' 2"	124"	44		FD3061
Engine 4R	207"	80"	9.5"	80"	34"	2.75"	9"	111"	31' 7"	116"		FD309R
Engine 5	221"	98.5"	8"	99.5"	9"			124"	34' 11"	100"		FD30136
Squad 4	205"	113"	18.5"	90"	12"	3"	11"	115"	33' 8"	112"		FD3035
Amb. 4	170"	79"	14.5"	35"	15"	3.5"	7.5"	107"	24'	116"		FD3011A
Amb. 5	170"	79"	14.5"	35"	15"	4.0"	8"	107"	24'	120"		FD2009A
Truck 3	274.5"	152.3"	23"	93.25"	15"		7.5"	140"	48.5'	118"		
Truck 16	270.5"	152.5"	23"	93.25"	15"		7.5"	140"	47' 9"	116"		45
Metra Eng.	186"	102.5"	17"	93.25"	17"							

\*\*F = The distance between the front fiddle axle stop and the front axle  
 \*\* = Vehicle Width includes mirrors

Date Collected 2022  
 Updated: 10/15/2019

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DATE
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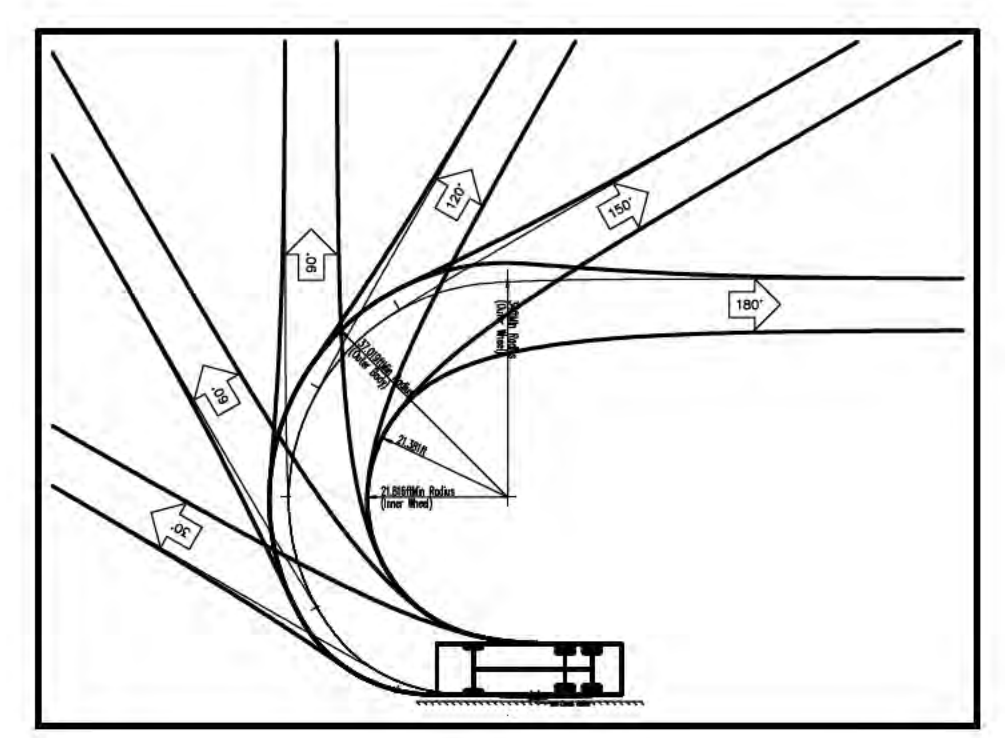
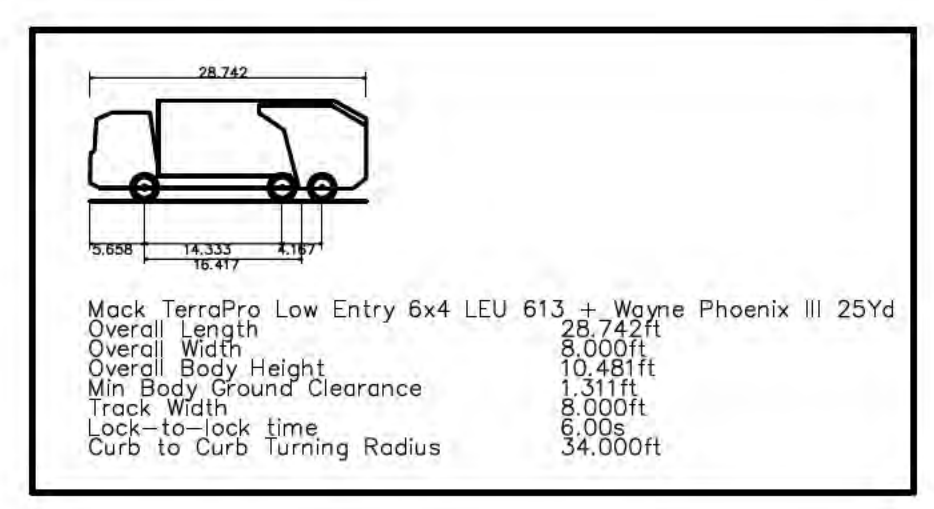
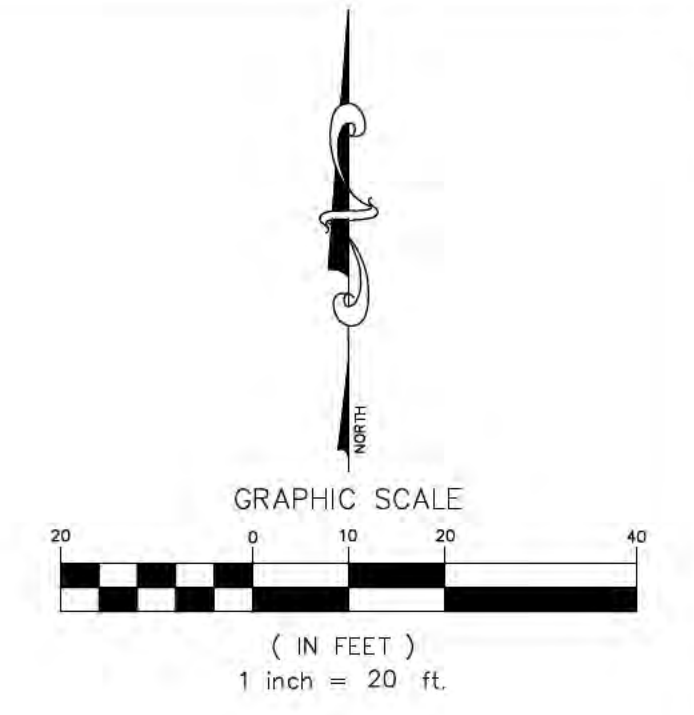
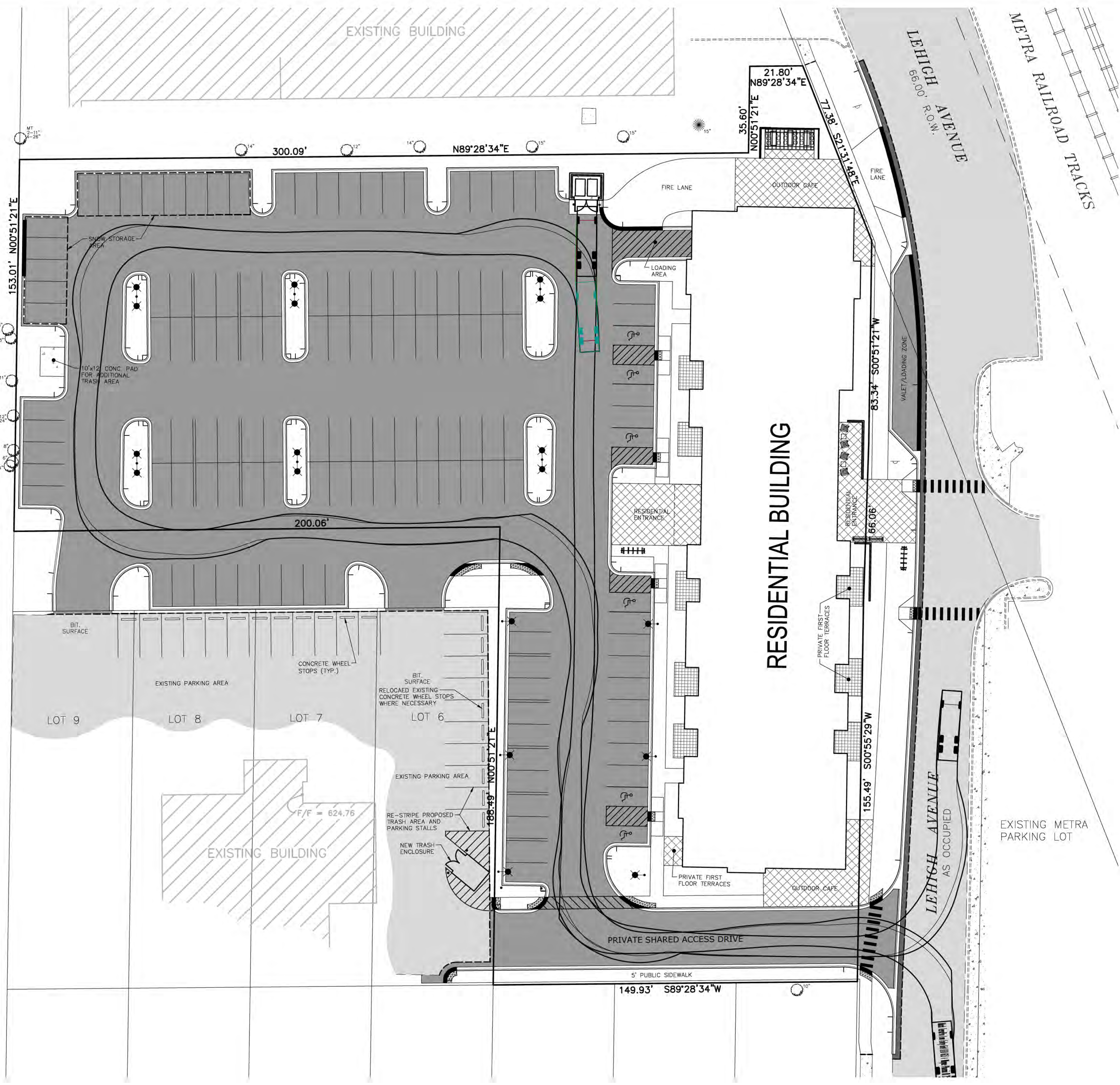
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 DATE 08/08/23  
 SCALE 1"=20'  
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SHEET  
**1** OF **1**



Photometric calculations are being provided to the recipient per the following disclaimer. This light level analysis is an estimate only, and is based on standard interior reflectance values of 0.8 ceilings, 0.5 walls, and 0.2 floors, unless otherwise specified. Any variance from reflectance values, obstructions, light loss factors or dimensional data will affect the actual light levels obtained. This analysis is a mathematical model and can only be as accurate as is permitted by the third party software and the IES files provided by our manufacturers.

Calculation by:  
Chris Collins  
Email:  
applications@amirep.com

REVISIONS  
COMMENTS

# DATE

Calculation For:

LETech

Project:  
**Morton Grove Apartments**

Date: 7/31/2025  
Scale: NTS Page 1 of 1



Luminaire Schedule			Mfr	Description	LLF	Luminaire Lumens	Luminaire Watts	Total Watts
Symbol	Qty	Tag						
□	6	P-4F-IS	LSI	VALS-09L-4F-40K7-IS	0.900	6198	54	324
□ □	6	P-5Q + P-4F-IS (2 @ 180)	LSI	VALS-09L-5Q-40K7 + VALS-09L-4F-40K7-IS (2@180) + 25' POLE	0.900	24310	162	972

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Plane @
PROPERTY_LINE	Illuminance	Fc	0.69	2.5	0.0	N.A.	N.A.
SITE	Illuminance	Fc	2.18	6.0	0.0	N.A.	0
NORTH_PARKING_GROUND	Illuminance	Fc	2.96	6.0	1.0	2.96	
SOUTH_PARKING_GROUND	Illuminance	Fc	2.37	3.5	1.4	1.69	

## MEMORANDUM

To: **Mr. Steve Spinell**  
Kinzie Real Estate Services, LLC

From: Justin Opitz, AICP  
Sofia Camp, EIT

Date: August 8, 2025

Subject: Mixed-Use Transit Oriented Development (TOD)  
8500-8550 Lehigh Avenue  
Morton Grove, Illinois

---

Kimley-Horn, Inc. (KH) was engaged to evaluate the traffic and parking characteristics of a proposed mixed-use development to be located at 8500-8550 Lehigh Avenue in Morton Grove, Illinois.

The proposed development would be located across Lehigh Avenue from the Morton Grove Metra station, providing service between Grayslake and Downtown Chicago via the MD-N Metra rail line. Given its proximity to transit, the development is considered a transit-oriented development (TOD), and, as such, its traffic and parking patterns may differ from other developments with the same land uses.

This memorandum estimates the anticipated traffic and parking patterns of the proposed TOD to determine any potential impacts to the surrounding road network or any deficiencies in the capacity of the proposed site parking lot.

### Site Characteristics

The proposed mixed-use TOD would be a multi-family residential building with ground floor commercial space, with the following composition of land uses:

- 60 residential units
- 3,000 square-foot restaurant
- 1,100 square-foot cafe

As part of the plan, Chestnut Street west of Lehigh Avenue would be removed, and a new full-access drive approximately 200 feet south of Chestnut Street would provide shared access to the proposed TOD and the existing Morton Grove Moose Lodge located directly southwest of the site.

The development would include a 124-space surface parking lot, including 97 spaces dedicated to the residents and visitors of the multi-family residential units, and 27 spaces dedicated to employees and patrons of the commercial developments.

Pertinent attachments, including the proposed development site plan and supporting documentation, are included as attachments.

## Traffic Evaluation

Kimley-Horn estimated site traffic for comparison to recent daily traffic volume data along Lehigh Avenue by the Illinois Department of Transportation (IDOT). This evaluation is qualitative in nature and is meant to provide context around the development's potential traffic impact.

## ITE Trip Generation

In order to estimate trip generation for the proposed site, data was referenced from the Institute of Transportation Engineers (ITE) manual titled Trip Generation, Eleventh Edition. Site traffic was calculated using trip generation rates for the ITE Land Use Codes (LUCs) corresponding to the land uses comprising the proposed development. A copy of the ITE trip generation data is provided as an attachment.

Further, given the context of the site location and development characteristics, site generated trips are expected to exhibit multiple routing patterns when traveling to and from the site, as described below:

- **Non-Auto** – Non-auto traffic represents trips generated via alternative modes of transportation, such as transit networks. Based on the site's location 350 feet away from a Metra rail station and supporting US census data, 15% of the trips generated were assumed to be transit-oriented. Relevant US census data is provided as an attachment.
- **Internal** – Internal trips represent movements between two land uses within the study area. According to data from the ITE Trip Generation Handbook, 3<sup>rd</sup> Edition, internally captured trips between the residential and commercial uses could represent approximately 10 percent of site generated trips.
- **Pass-by** – Pass-by traffic represents motorists who are already traveling on the adjacent study roadways and stop at the site en route to another destination. While ITE's Trip Generation Manual, 11<sup>th</sup> Edition, does not provide pass-by data for a Café (LUC 936), the data for High-Turnover (Sit-Down) Restaurant (LUC 932) indicates that roughly 43 percent of vehicles are pass-by trips during the weekday evening peak hour. To maintain a conservative estimate, a 20 percent pass-by rate was assumed for the restaurant and café trips, which is consistent with the maximum pass-by reduction that is typically recommended by IDOT.

As summarized in **Table 1**, the trip generation estimates were calculated for weekday daily, morning peak hour, and evening peak hour times using the ITE data provided as an attachment.

**Table 1. Site Trip Generation (ITE Data)**

Land Use	Size	Weekday Site Trips						
		Daily <sup>1</sup>	AM Peak Hour			PM Peak Hour		
			In	Out	Total	In	Out	Total
Multi-Family Housing (Low-Rise) – LUC 220	60 DU	460	10	32	42	29	17	46
Café – LUC 936 <sup>2</sup>	1,100 SF	500	52	50	102	18	18	36
Restaurant – LUC 932	3,000 SF	320	16	13	29	17	11	28
<b>Subtotal</b>		<b>1,280</b>	<b>78</b>	<b>95</b>	<b>173</b>	<b>64</b>	<b>46</b>	<b>110</b>
<i>Less Non-Auto Trips<sup>3</sup></i>	<i>-15%</i>	<i>-200</i>	<i>-12</i>	<i>-15</i>	<i>-27</i>	<i>-10</i>	<i>-8</i>	<i>-18</i>
<i>Less Internal Capture<sup>3</sup></i>	<i>-10%</i>	<i>-120</i>	<i>-8</i>	<i>-8</i>	<i>-16</i>	<i>-4</i>	<i>-4</i>	<i>-8</i>
<i>Less Pass-by Trips<sup>4</sup></i>	<i>-20%</i>	<i>-160</i>	<i>-13</i>	<i>-13</i>	<i>-26</i>	<i>-7</i>	<i>-7</i>	<i>-14</i>
<b>Total New Trips</b>		<b>800</b>	<b>45</b>	<b>59</b>	<b>104</b>	<b>43</b>	<b>27</b>	<b>70</b>

<sup>1</sup> Daily trips rounded to the nearest multiple of 10

<sup>2</sup> No ITE data is available for Weekday Daily Café (LUC 936) trips and thus estimates were based on Fast-Food Restaurant without Drive Through Window (LUC 933)

<sup>3</sup> Non-Auto and Internal Capture trip reductions applied to all development land uses

<sup>4</sup> Pass-By trip reduction applied only to Café and Restaurant.

### Traffic Evaluation

Kimley-Horn obtained traffic volumes along Lehigh Avenue from IDOT's Traffic Count Database System (TCDS) using the most recent year (2023) for bi-directional through movement traffic counts. Additionally, four recently approved developments along Lehigh Avenue were incorporated into this analysis. Based on the referenced TCDS counts, previous analyses conducted by Kimley-Horn for the approved developments, and the trip generation estimates in Table 2 above, **Table 3** summarizes existing and future traffic conditions on Lehigh Avenue. The IDOT count data is provided as an attachment.

**Table 2. Existing and Future Traffic Volume Comparison**

Future Development	Weekday		
	Daily	AM Peak	PM Peak
<b>Existing Volumes</b>			
Existing Counts <sup>1</sup>	3,573	223	320
<b>Approved Development Trip Generation<sup>2</sup></b>			
GIL Sewing Corporation + Pickleball Facility (6451 Main Street)	736	105	111
Metro on Main Residential Development (Lehigh Avenue/Main Street)	640	45	50
Badminton Gym/Training Facility (8150 Lehigh Avenue)	240	-- <sup>3</sup>	100
Pediatric Therapy Office (8210 Lehigh Avenue)	74	16	14
<b>Subtotal EXISTING + APPROVED</b>	<b>5,263</b>	<b>389</b>	<b>595</b>
<b>Proposed Development Trip Generation</b>			
Mixed-Use Transit-Oriented Development (8500-8550 Lehigh Avenue)	930	104	66
<b>Future Total EXISTING + FUTURE APPROVED + PROPOSED</b>	<b>6,193</b>	<b>493</b>	<b>661</b>

<sup>1</sup> Referenced from IDOT’s Traffic Count Database System

<sup>2</sup> Referenced from previous studies conducted by Kimley-Horn

<sup>3</sup> The future badminton gym is not planned to have hours of operation during the morning peak hour.

Per the Highway Capacity Manual (HCM), Lehigh Avenue has a capacity of about 10,000 vehicles per day before experiencing significant congestion and delay. As highlighted in Table 2, with the addition of the proposed mixed-use development traffic, Lehigh Avenue is anticipated to carry approximately 6,193 vehicles per day. Thus, Lehigh Avenue would have capacity for approximately 3,800 additional daily trips before segments of the roadway reach their limit for efficient traffic operations.

Additionally, the projected site trips would increase the traffic along Lehigh Avenue to 493 and 661 trips in the morning and evening peak hours, respectively. In other words, this level of traffic equates to roughly 8-11 vehicles per minute in the peak hours.

Based on the traffic projections, Lehigh Avenue is anticipated to accommodate the site traffic without material impacts to its operations.

### Similar Development Comparison

Since ITE trip generation rates are based on data collected nationwide, they often do not fully reflect context- and location-specific traffic patterns. Kimley-Horn conducted 24-hour driveway counts at The Reserve at Glenview, a residential TOD similar to the proposed development, located adjacent to the Golf Metra station in Glenview, Illinois. The development consists of 239 multi-family units with access provided via two full-access driveways on Overlook Drive. The driveway counts were used to develop daily, morning peak hour, and afternoon peak hour trip

generation rates that may be more representative of the residential site traffic for the proposed development.

**Table 3** summarizes the entry and exit data at The Reserve at Glenview and calculates a per-residential unit trip generation rate for the site, which was then applied to the residential portion of the proposed development in Morton Grove to obtain an overall trip generation estimate for the site based on local data.

**Table 3. Site Trip Generation (Local Residential Data)**

Description	Size	Weekday Site Trips						
		Daily <sup>1</sup>	AM Peak Hour			PM Peak Hour		
			In	Out	Total	In	Out	Total
<b>The Reserve at Glenview</b>								
Multifamily Residential – Local Data	239 DU	1210	36	54	90	60	37	97
Trip Generation Rate	Per DU	5.1	40%	60%	0.38	62%	38%	0.41
<b>Proposed Development</b>								
Multifamily Residential – Local Data	60 DU	310	9	14	23	15	9	24
Café – LUC 9361	1,100 SF	500	52	50	102	18	18	36
Restaurant – LUC 932	3,000 SF	320	16	13	29	17	11	28
	<b>Subtotal</b>	<b>1,130</b>	<b>77</b>	<b>77</b>	<b>154</b>	<b>50</b>	<b>38</b>	<b>88</b>
	<i>Less Non-Auto Trips<sup>3</sup></i>	<i>-15%</i>	<i>-120</i>	<i>-10</i>	<i>-9</i>	<i>-19</i>	<i>-5</i>	<i>-4</i>
	<i>Less Captive Trips<sup>3</sup></i>	<i>-10%</i>	<i>-80</i>	<i>-7</i>	<i>-6</i>	<i>-13</i>	<i>-3</i>	<i>-6</i>
	<i>Less Pass-by Trips<sup>3</sup></i>	<i>-20%</i>	<i>-160</i>	<i>-13</i>	<i>-13</i>	<i>-26</i>	<i>-6</i>	<i>-12</i>
	<b>Total New Trips</b>	<b>770</b>	<b>47</b>	<b>49</b>	<b>96</b>	<b>36</b>	<b>25</b>	<b>61</b>

<sup>1</sup> Daily trips rounded to the nearest multiple of 10

<sup>2</sup> No ITE data is available for Weekday Daily Café (LUC 936) trips and thus estimates were based on Fast-Food Restaurant without Drive Through Window (LUC 933)

<sup>3</sup> Non-Auto, Captive, and Pass-by trip reductions applied only to Café and Restaurant.

The trip generation rate developed from the local data from The Reserve at Glenview is lower than the rate provided for ITE’s multifamily residential LUC. As a result, the site traffic estimate for the residential portion of the proposed development, and therefore the overall projected site trip generation, is approximately 3 to 13 percent lower than the estimate using ITE data. As such, the conclusions provided in the Traffic Evaluation section on Page 4 remain applicable.

## Previous Development Plan Traffic Study

Kimley-Horn conducted a traffic and parking study, dated January 2022, for a previous proposed development at the subject location, whose plan included 24 multi-family residential units and approximately 9,700 square feet of ground-floor commercial space. Relative to this previous development plan, the new proposed plan includes 36 additional residential units (60 total units) and 5,600 fewer square feet of commercial space (4,100 total square feet)

**Table 4** compares the trip generation for the previous and new development plans.

**Table 4. Previous and New Development Plan Trip Generation Comparison**

Land Uses	Weekday						
	Daily	AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
Previous Development Plan (24 DU + 9,700 SF ground-floor commercial space)	890	47	53	100	54	39	93
New Development Plan (60 DU + 4100 SF ground-floor commercial space)	800	45	59	104	43	27	70
<b>Net Change</b>	<b>-90</b>	<b>-2</b>	<b>+6</b>	<b>+4</b>	<b>-11</b>	<b>-12</b>	<b>-23</b>

<sup>1</sup> Daily trips rounded to the nearest multiple of 10

As compared to the previous development plan, the new development is projected to generate 10 percent less daily trips, 4 percent more trips in the morning peak hour, and 25 percent less trips in the evening peak hour.

## Parking Evaluation

Parking at the mixed-used TOD at 8500-8550 Lehigh Avenue is planned to be provided via a parking lot located on the western portion of the site. The mixed-use building plans to provide 124 overall off-street parking spaces, including six dedicated ADA spaces, of which 97 are dedicated to residents of the multi-family housing and 27 are dedicated to employees and patrons of the ground-floor commercial tenants.

Kimley-Horn reviewed the following two metrics to evaluate the adequacy of the proposed parking supply for the mixed-use TOD:

1. Parking requirements based on Village of Morton Grove Unified Development Code.
2. Estimated parking demand based on national industry resources.

### Part 1. Village Code Requirements

**Table 5** presents the parking requirements per the Village of Morton Grove Unified Development Code for the uses in the proposed plan. The Village code defines parking requirements based on dwelling units and SF of gross floor area for the multi-family housing and commercial uses, respectively.

Section 12-5-7:2 of the Village code permits a 15 percent reduction in parking spaces for TOD developments within 1/8 mile (660 feet) of a commuter rail station. The proposed development is located approximately 350 feet from the Morton Grove Metra Station, and therefore is eligible for the parking requirement reduction.

**Table 5. Village Code Off-Street Parking Requirements**

Code Categorization / Land Use	Size	Required Space by Use	Required Spaces	Required Spaces (Less 15% TOD Reduction)
Multi-Family Housing (Low-Rise)	60 DU	1.75 space per dwelling unit plus 0.15 space per dwelling unit for guest parking <sup>1</sup>	114 (105 + 9 guest)	97 (89 + 8 guest)
Café	1,100 SF	1.0 space per 150 square feet of gross area	7	6
Restaurant	3,000 SF	1.0 space per 150 square feet of gross area	20	17
<b>Total Required Off-Street Parking Spaces</b>			<b>141</b>	<b>120</b>

<sup>1</sup> Guest parking required for developments with greater than 10 multi-family housing units

Based on Village code requirements for a TOD, a total of 120 off-street parking spaces are required, including 97 (89 resident spaces and 8 guest spaces) for the proposed multi-family housing units, 6 for the café, and 17 for the restaurant.

As such, the proposed supply of 124 off-street parking spaces exceeds the Village requirement for the overall development by four spaces. The development plan meets the individual

requirement for residential parking supply and exceeds the requirement for commercial parking by four spaces.

### Part 2. National Industry Resources

Kimley-Horn reviewed parking demand data provided in the Institute of Transportation Engineers (ITE) Parking Generation Manual – 6<sup>th</sup> Edition to determine off-street parking demand for the proposed plan per industry research. A copy of the ITE parking generation data is provided as an attachment.

**Table 6. ITE Peak Parking Demand Projections**

Tenant	ITE Land Use	Size	Average Peak Demand			
			Weekday		Saturday	
			Rate	Projection	Rate	Projection
Multi-Family Housing	Multi-Family Housing (Low-Rise) – LUC 220	60 DU	S = 1.27(X)	77 spaces	S = 1.18(X)	71 spaces
Café	Coffee/Donut Shop without Drive Through – LUC 936	1,100 SF	S = 10.36(X)	12 spaces	S = 14.44(X)	16 spaces
Restaurant	High-Turnover (Sit-Down) Restaurant – LUC 932	3,000 SF	S = 8.97(X)	27 spaces	S = 11.50(X)	34 spaces
<b>Total Average Peak Parking Demand</b>			<b>116 spaces</b>		<b>121 spaces</b>	

S = number of spaces      X = Dwelling Units or 1,000 SF GFA

As shown in **Table 6**, based on ITE parking demand data, the proposed parking supply of 124 spaces meets the average peak demand for the overall development with a surplus of eight parking spaces during the weekday and three spaces on Saturday.

The ITE demand projections for the café and restaurant exceed the supply of spaces currently allocated to the commercial uses (27 spaces), while the projected residential parking demand is below its designated supply (97 spaces) by up to 16 spaces. As the development becomes occupied, parking demand among the various uses should be monitored, and parking space designations can be adjusted accordingly. Further, the transit-oriented nature of the development may reduce parking demand, as the ITE projections are based on data that is not specific to transit-oriented developments.

## Conclusion

Kimley-Horn evaluated the traffic projections, parking requirements, and projected operations of the proposed mixed-use transit-oriented development to be located 8500-8550 Lehigh Avenue.

### Traffic Evaluation

The site traffic expected to be added to Lehigh Avenue is not anticipated to significantly impact operations along the study area roadway. Installation of pedestrian crosswalk striping along the south leg of the Lehigh Avenue and Chestnut Avenue intersection should be considered as the residents of the proposed multi-family housing would use this intersection to access the adjacent Morton Grove Metra station.

Outbound traffic at the proposed private shared access drive serving the mixed-use TOD should operate under minor-leg stop control operations with Lehigh Avenue and provide a stop sign, stop bar, and pedestrian crosswalk striping along the west leg. As the site design progresses, care should be taken with landscaping, signage, and monumentation at the private street access with Lehigh Avenue to ensure that adequate horizontal distance is maintained.

### Parking Evaluation

The proposed supply of 124 off-street parking spaces exceeds the Village requirement for the overall development by four spaces. The development plan meets the individual requirement for residential parking supply and exceeds the requirement for commercial parking by four spaces. Further, the proposed parking supply is projected to accommodate the average peak parking demand for the overall development based on data from ITE's Parking Generation Manual, 6<sup>th</sup> Edition.

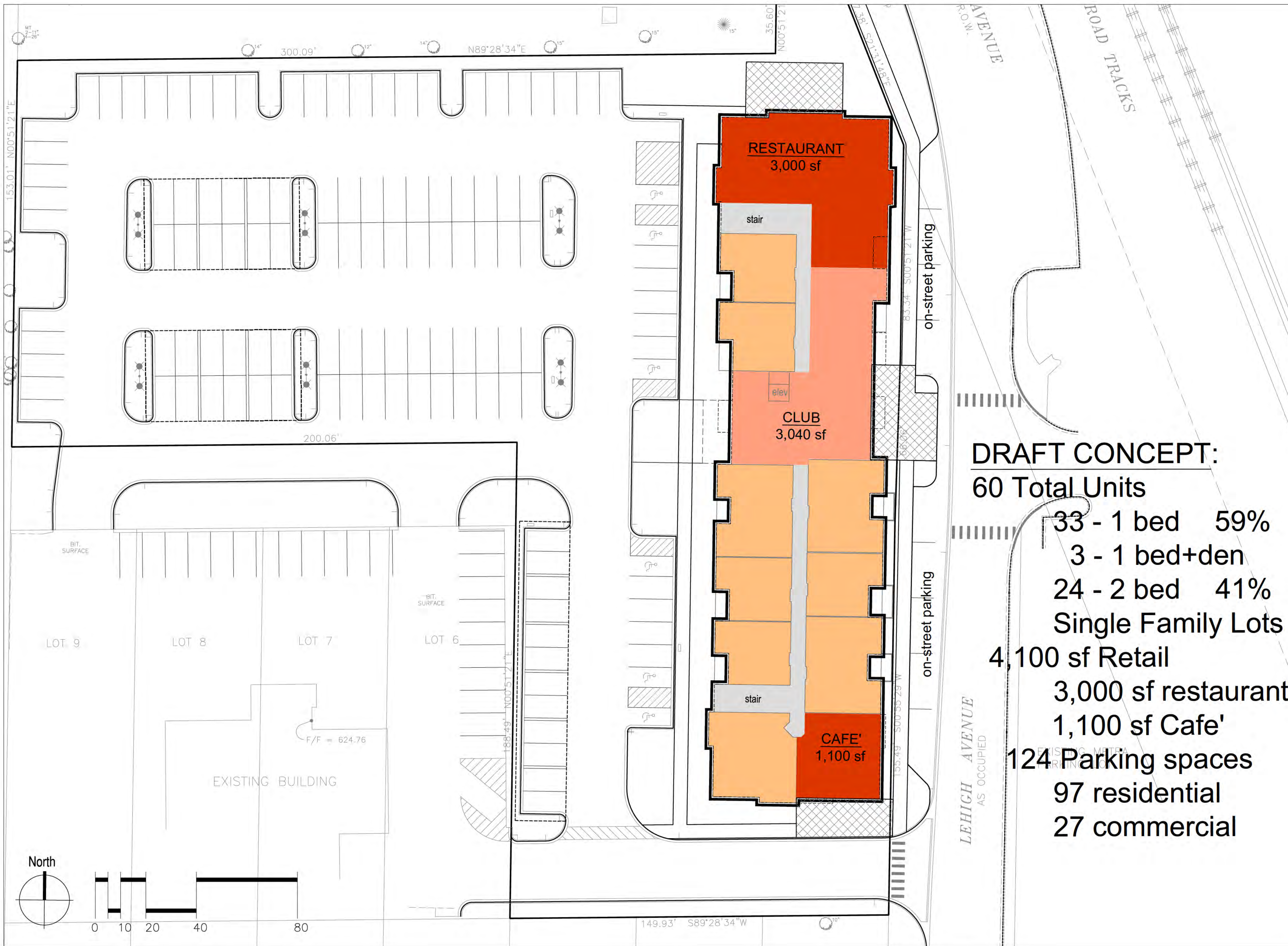
As the development becomes occupied, parking demand for each of the uses should be monitored, and parking designations can be updated accordingly to accommodate actual demand.

## ATTACHMENTS

1. Site Plan
2. ITE Trip Generation Manual, 11<sup>th</sup> Edition Excerpts
3. IDOT Traffic Volume Data
4. ITE Parking Generation Manual, 6<sup>th</sup> Edition Excerpts



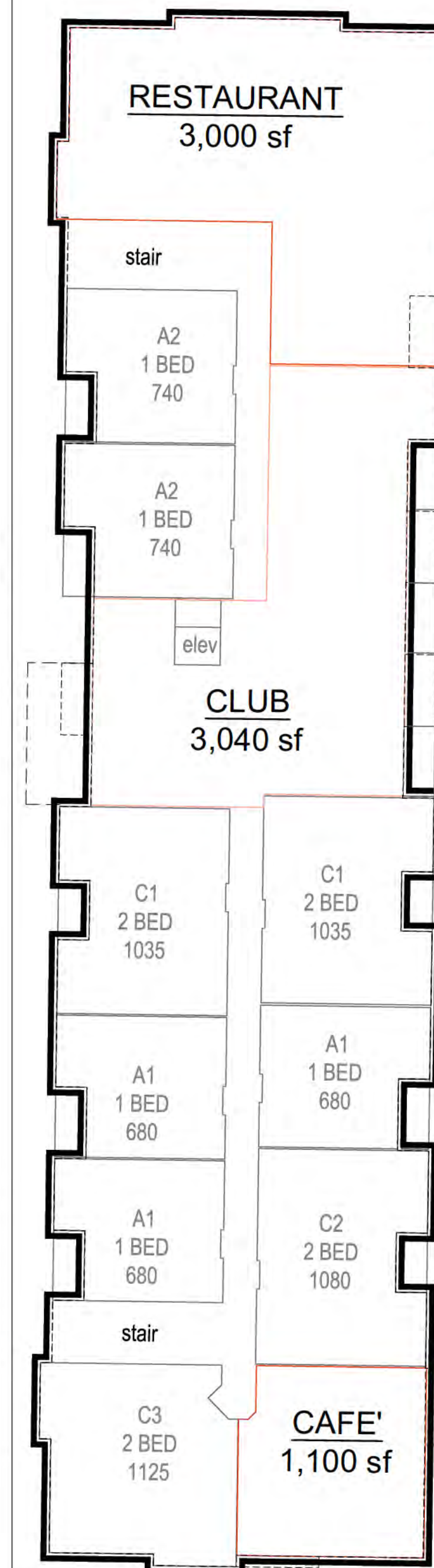
SITE PLAN



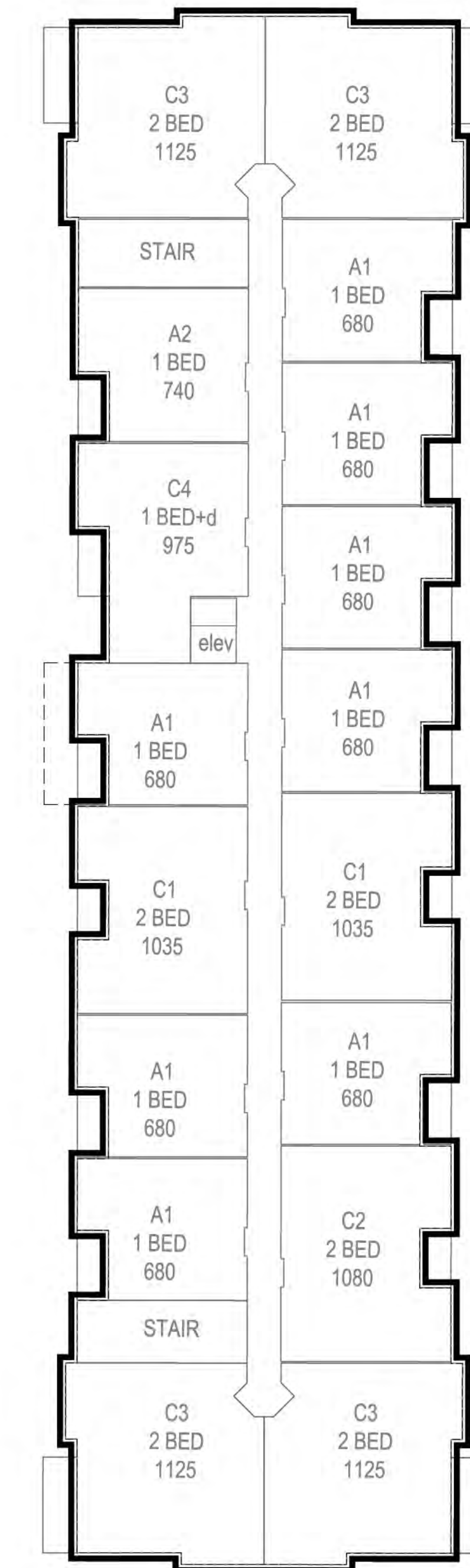
**DRAFT CONCEPT:**  
60 Total Units

- 33 - 1 bed 59%
- 3 - 1 bed+den
- 24 - 2 bed 41%
- Single Family Lots

- 4,100 sf Retail
- 3,000 sf restaurant
- 1,100 sf Cafe'
- 124 Parking spaces
- 97 residential
- 27 commercial



**FLOOR 1**



**FLRS 2, 3, 4**

Site Illustrative Plan

Floor Plates



StationView  
Morton Grove, Illinois



April 28, 2025



ITE TRIP GENERATION MANUAL, 11TH EDITION EXCERPTS

# Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

Vehicle Trip Ends vs: Dwelling Units  
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 22

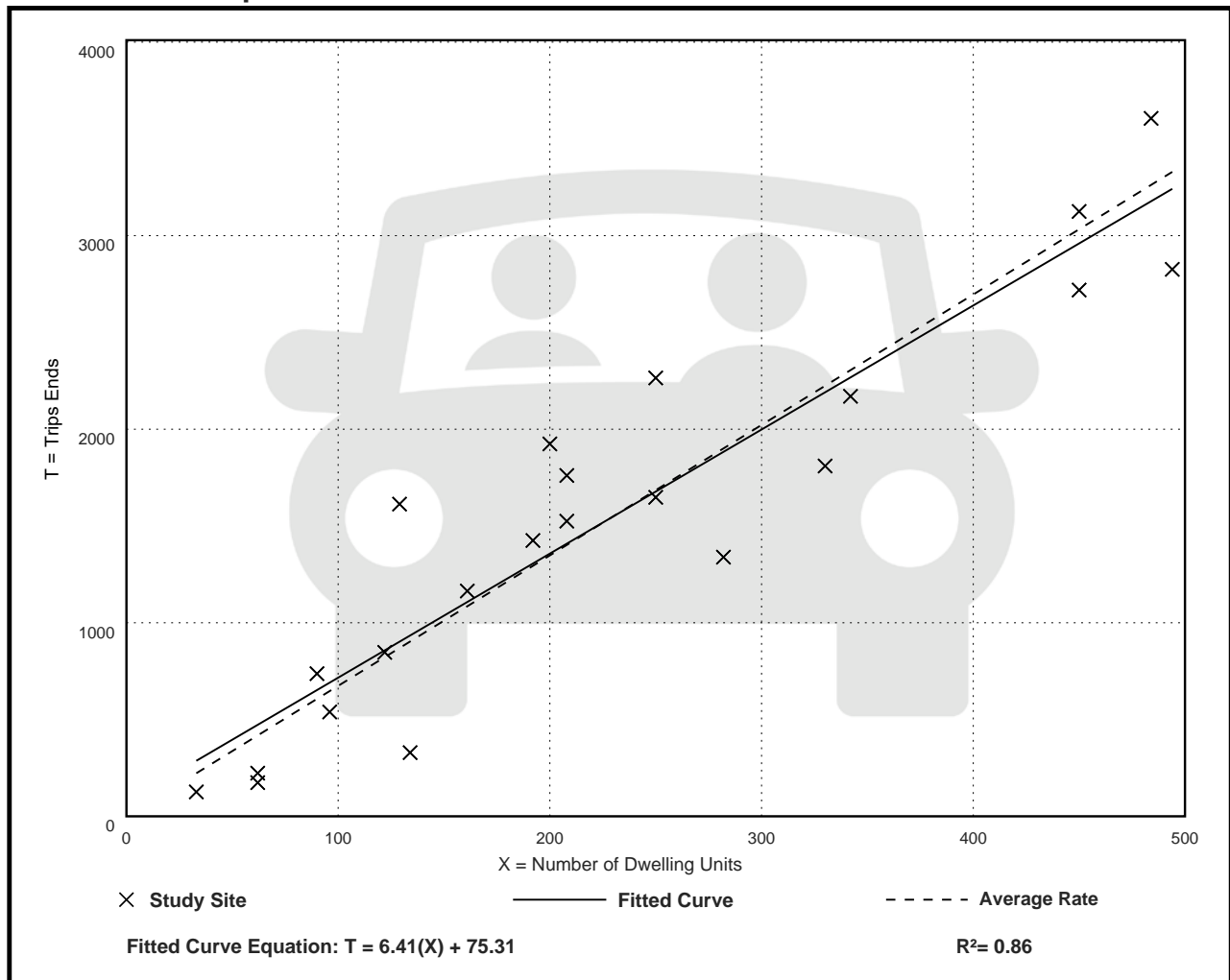
Avg. Num. of Dwelling Units: 229

Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
6.74	2.46 - 12.50	1.79

## Data Plot and Equation



# Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

## Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 49

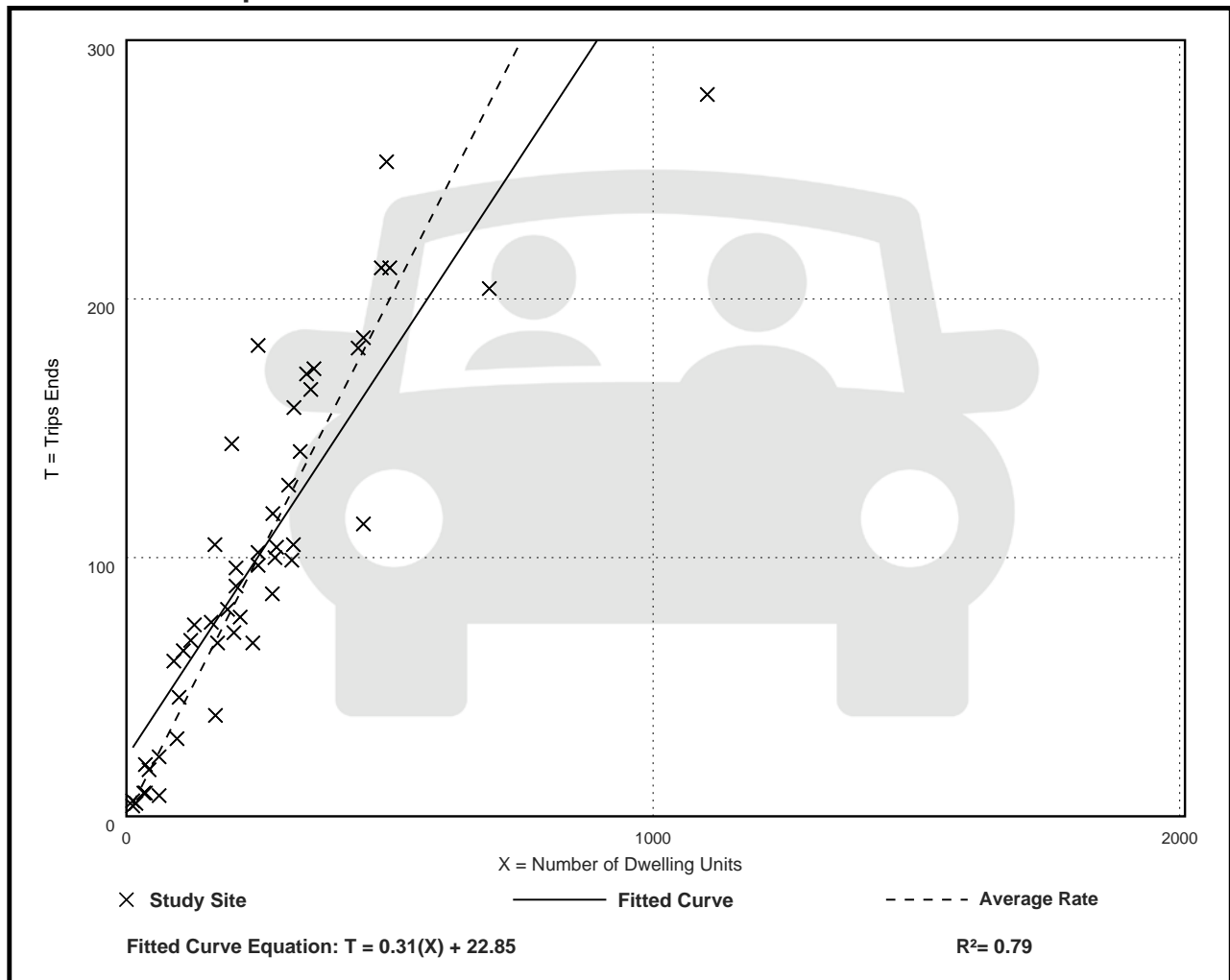
Avg. Num. of Dwelling Units: 249

Directional Distribution: 24% entering, 76% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.40	0.13 - 0.73	0.12

## Data Plot and Equation



# Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 59

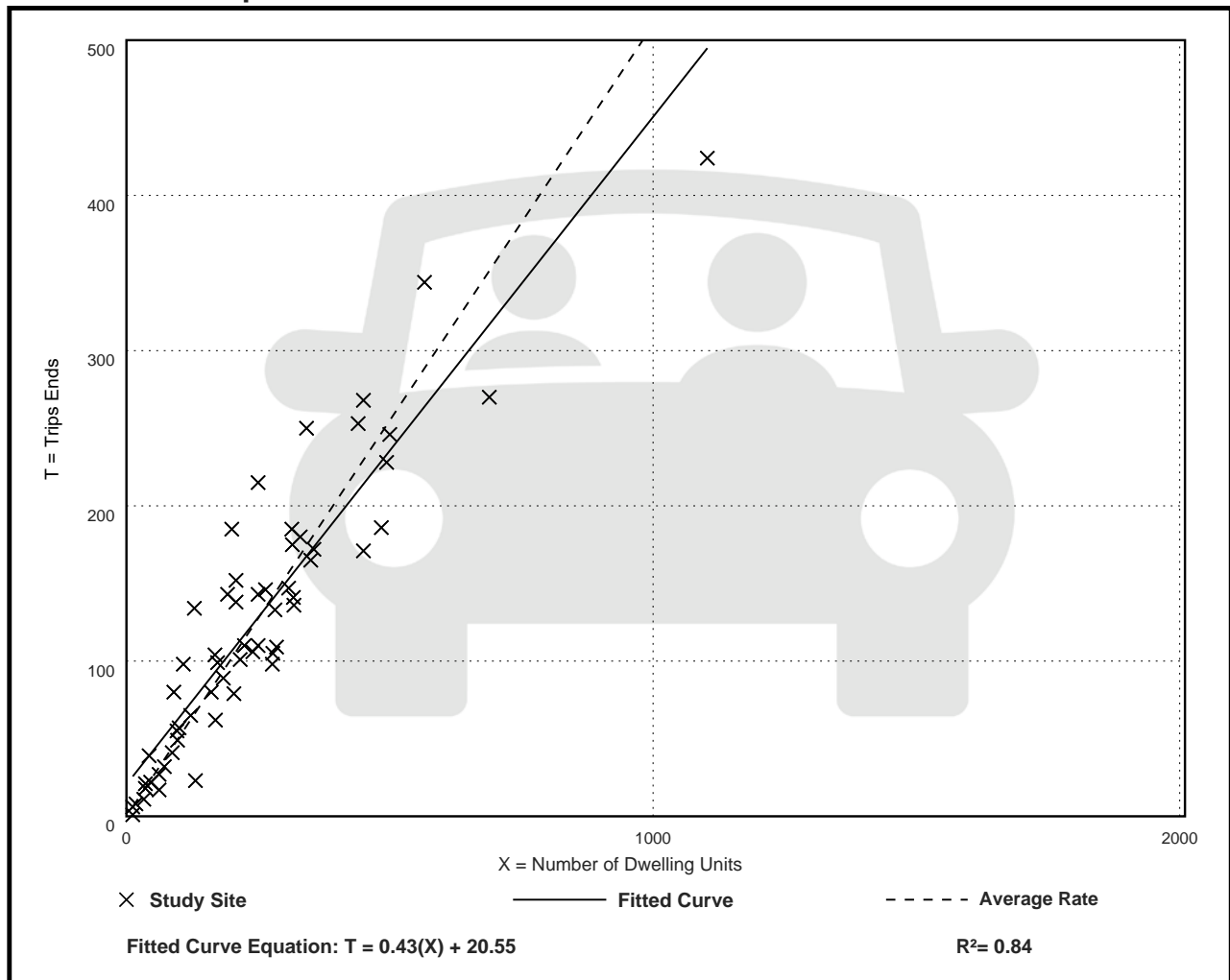
Avg. Num. of Dwelling Units: 241

Directional Distribution: 63% entering, 37% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.51	0.08 - 1.04	0.15

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA  
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 50

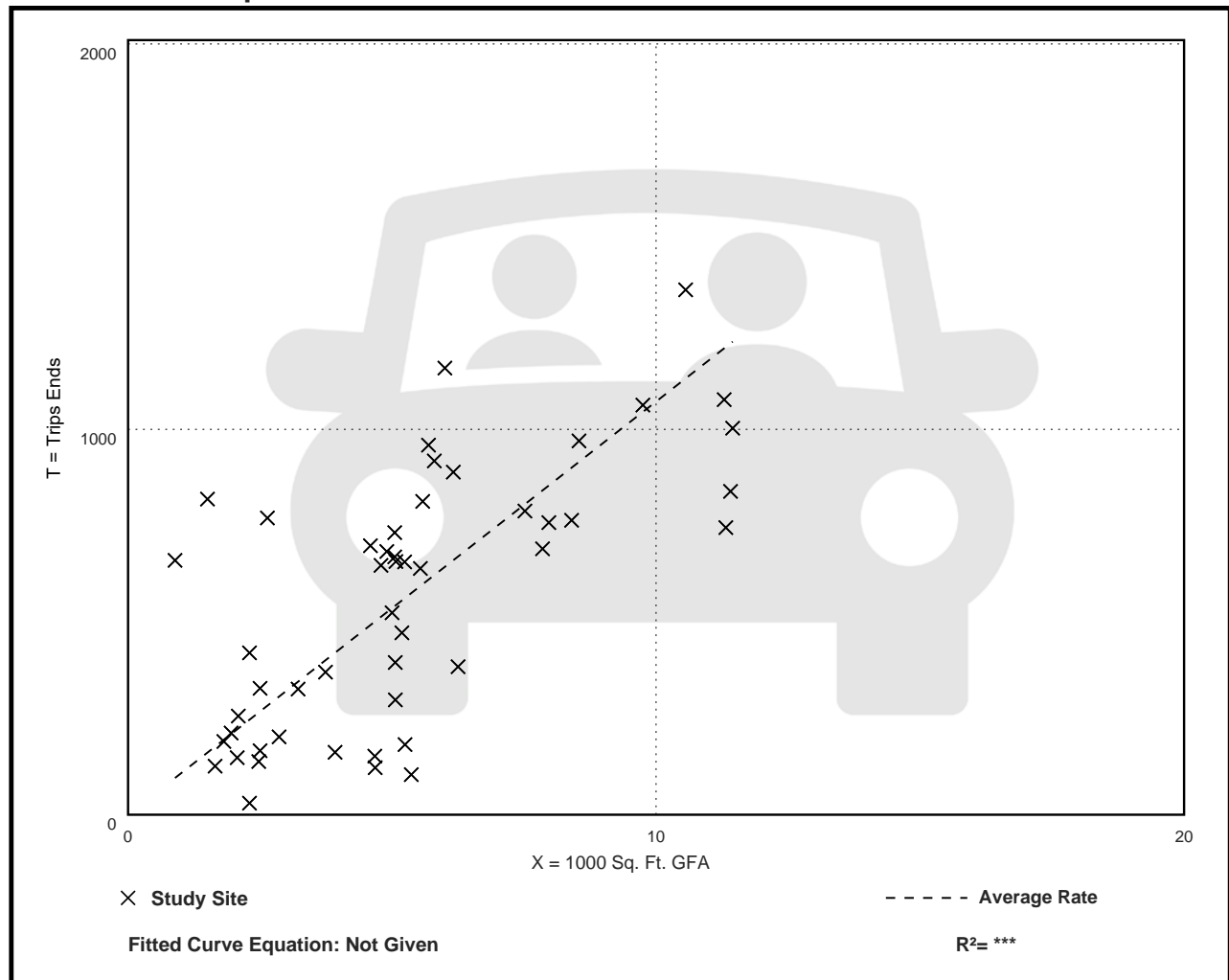
Avg. 1000 Sq. Ft. GFA: 5

Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
107.20	13.04 - 742.41	66.72

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**

**On a: Weekday,**

**Peak Hour of Adjacent Street Traffic,**

**One Hour Between 7 and 9 a.m.**

**Setting/Location: General Urban/Suburban**

Number of Studies: 37

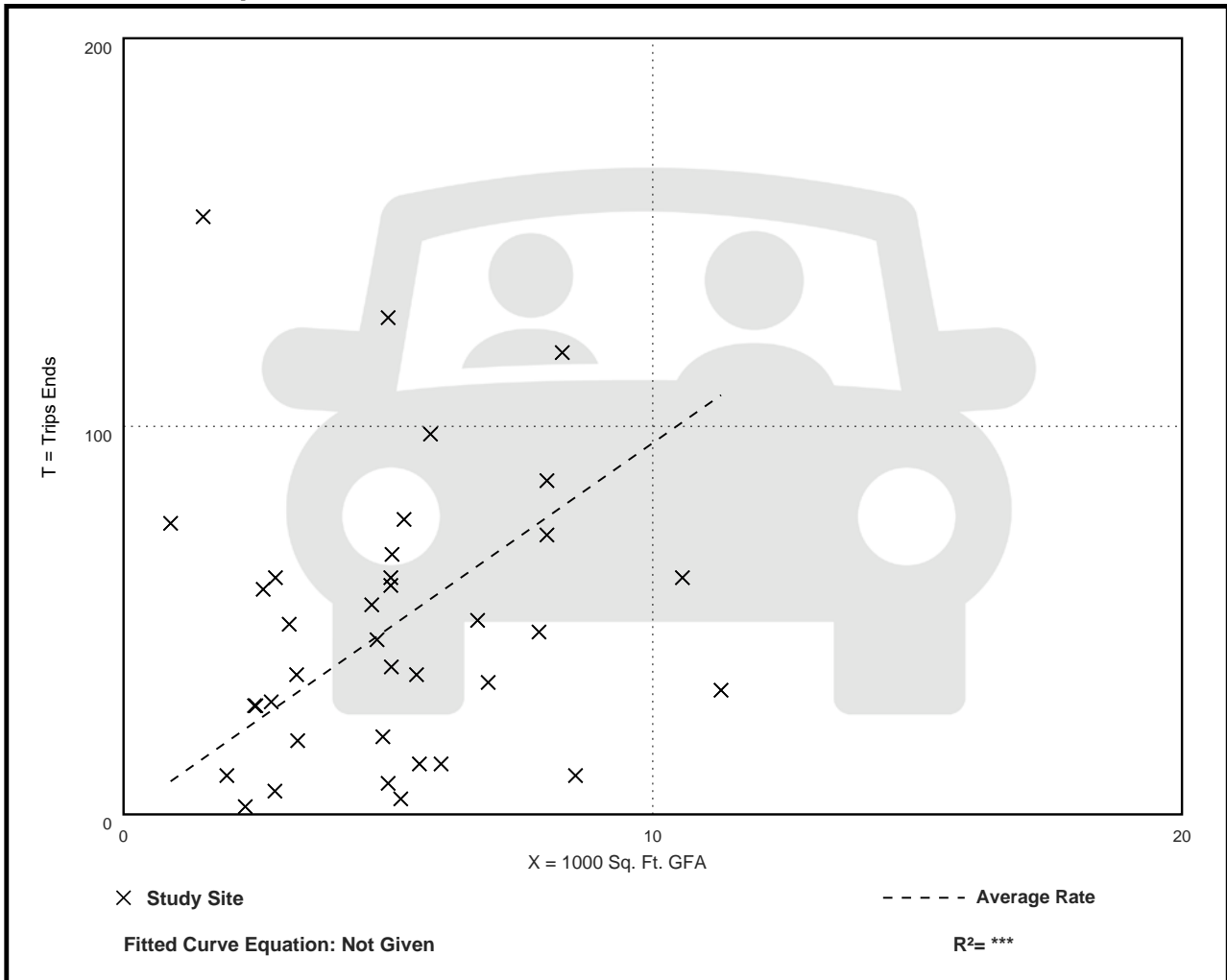
Avg. 1000 Sq. Ft. GFA: 5

Directional Distribution: 55% entering, 45% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.57	0.76 - 102.39	11.61

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 104

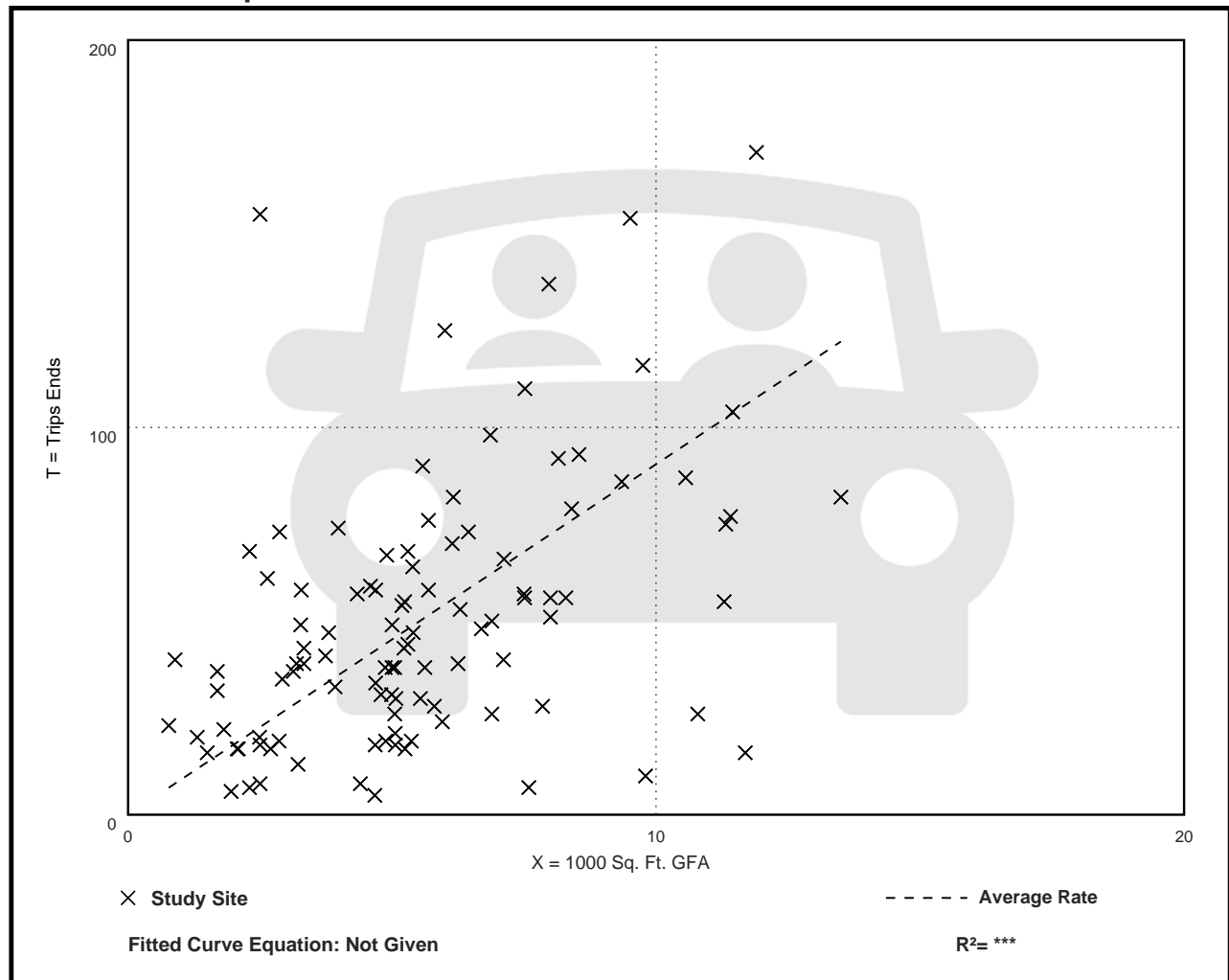
Avg. 1000 Sq. Ft. GFA: 6

Directional Distribution: 61% entering, 39% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.05	0.92 - 62.00	6.18

## Data Plot and Equation



# Coffee/Donut Shop without Drive-Through Window (936)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 25

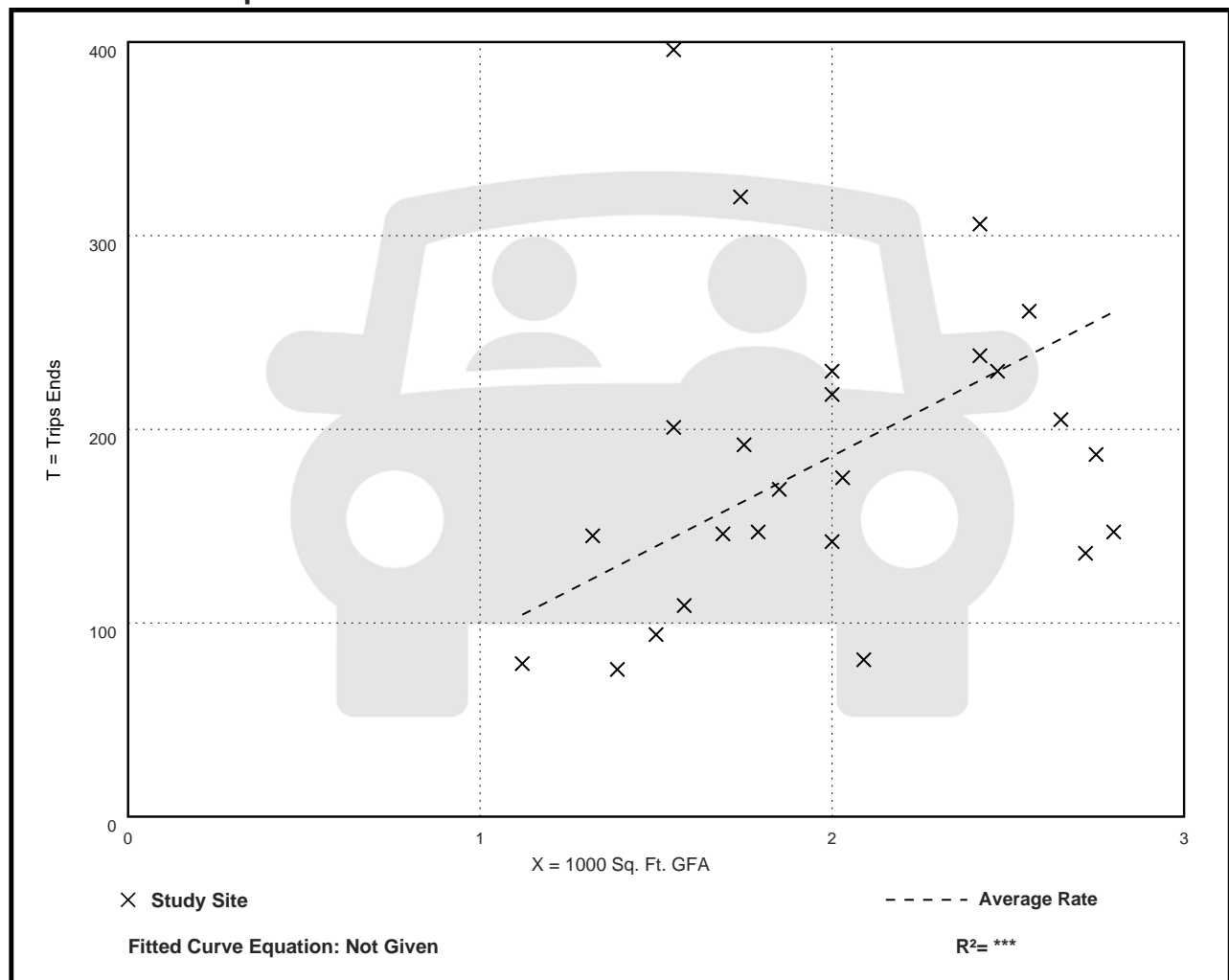
Avg. 1000 Sq. Ft. GFA: 2

Directional Distribution: 51% entering, 49% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
93.08	38.76 - 255.48	42.71

## Data Plot and Equation



# Coffee/Donut Shop without Drive-Through Window (936)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 16

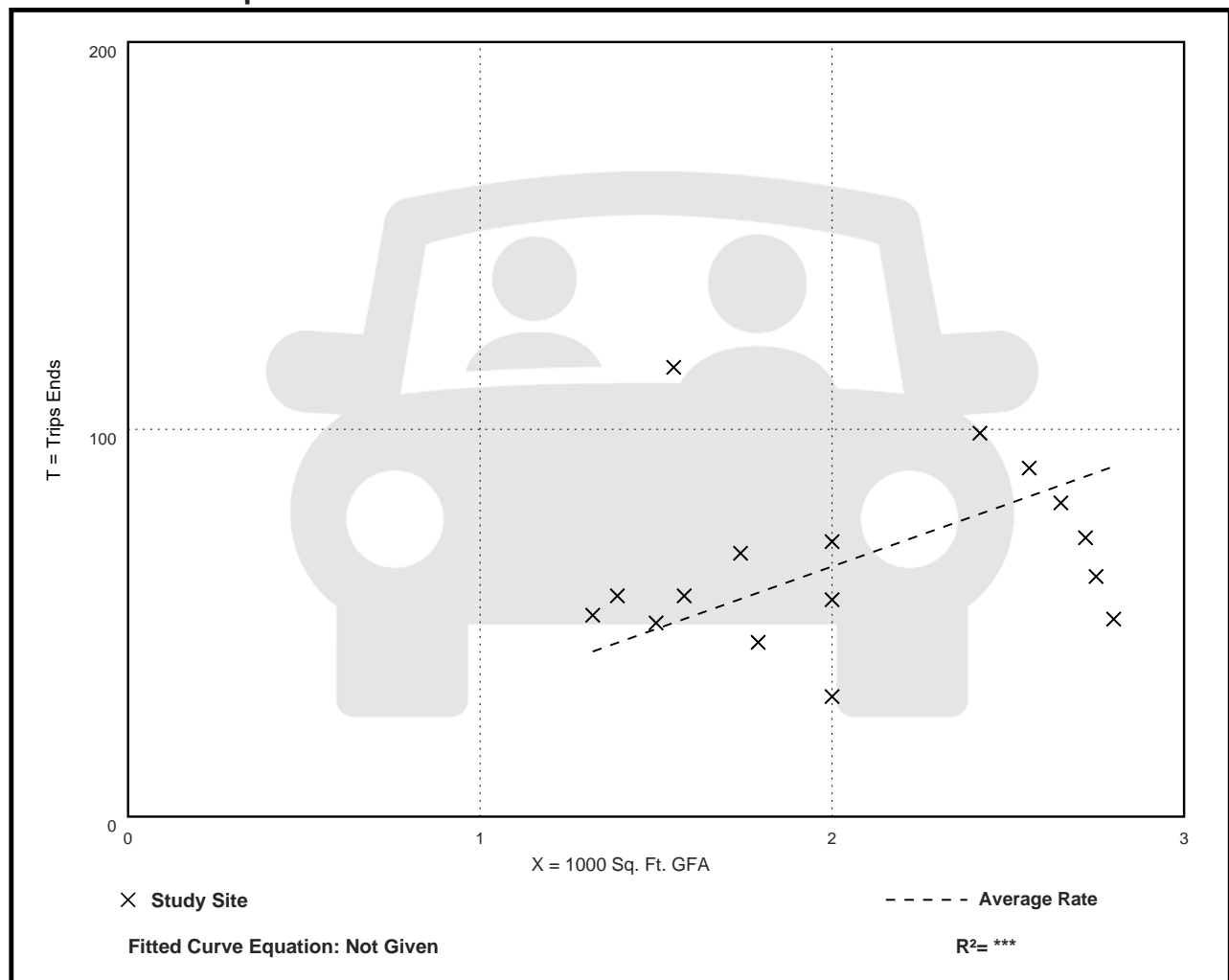
Avg. 1000 Sq. Ft. GFA: 2

Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
32.29	15.50 - 74.84	12.64

## Data Plot and Equation





# Means of Transportation to Work by Vehicles Available



Note: This is a modified view of the original table produced by the U.S. Census Bureau. This download or printed version may have missing information from the original table.

## Morton Grove village, Illinois

Label	Estimate	Margin of Error
▼ Total:	11,874	±607
No vehicle available	236	±134
1 vehicle available	1,663	±406
2 vehicles available	5,775	±832
3 or more vehicles available	4,200	±635
➤ Car, truck, or van - drove alone:	8,006	±507
➤ Car, truck, or van - carpooled:	1,175	±262
➤ Public transportation (excluding taxicab):	522	±157
➤ Walked:	171	±114
➤ Taxicab, motorcycle, bicycle, or other means:	146	±75
➤ Worked from home:	1,854	±322

# Table Notes

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## Means of Transportation to Work by Vehicles Available

**Survey/Program:** American Community Survey

**Universe:** Workers 16 years and over in households

**Year:** 2023

**Estimates:** 5-Year

**Table ID:** B08141

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, the decennial census is the official source of population totals for April 1st of each decennial year. In between censuses, the Census Bureau's Population Estimates Program produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units and the group quarters population for states and counties.

Information about the American Community Survey (ACS) can be found on the ACS website. Supporting documentation including code lists, subject definitions, data accuracy, and statistical testing, and a full list of ACS tables and table shells (without estimates) can be found on the Technical Documentation section of the ACS website.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the [Methodology](#) section.

Source: U.S. Census Bureau, 2019-2023 American Community Survey 5-Year Estimates

ACS data generally reflect the geographic boundaries of legal and statistical areas as of January 1 of the estimate year. For more information, see [Geography Boundaries by Year](#).

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

Users must consider potential differences in geographic boundaries, questionnaire content or coding, or other methodological issues when comparing ACS data from different years. Statistically significant differences shown in ACS Comparison Profiles, or in data users' own analysis, may be the result of these differences and thus might not necessarily reflect changes to the social, economic, housing, or demographic characteristics being compared. For more information, see [Comparing ACS Data](#).

Workers include members of the Armed Forces and civilians who were at work last week.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on 2020 Census data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

### Explanation of Symbols:

-

The estimate could not be computed because there were an insufficient number of sample observations. For a ratio of medians estimate, one or both of the median estimates falls in the lowest interval or highest interval of an open-ended distribution. For a 5-year median estimate, the margin of error associated with a median was larger than the median itself.

N

The estimate or margin of error cannot be displayed because there were an insufficient number of sample cases in the selected geographic area.

(X)

The estimate or margin of error is not applicable or not available.

median-

The median falls in the lowest interval of an open-ended distribution (for example "2,500-")

median+

The median falls in the highest interval of an open-ended distribution (for example "250,000+").

\*\*

The margin of error could not be computed because there were an insufficient number of sample observations.

\*\*\*

The margin of error could not be computed because the median falls in the lowest interval or highest interval of an open-ended distribution.

\*\*\*\*

A margin of error is not appropriate because the corresponding estimate is controlled to an independent population or housing estimate. Effectively, the corresponding estimate has no sampling error and the margin of error may be treated as zero.

IDOT TRAFFIC VOLUME DATA



# Volume Count Report

LOCATION INFO	
Location ID	016 1212
Type	LINK
Funct'l Class	5
Located On	Lehigh Ave
From Road	Lincoln Ave
To Road	Oakton St
Direction	2-WAY
County	Cook
Community	MORTON GROVE
MPO ID	
HPMS ID	
Agency	Illinois DOT

COUNT DATA INFO	
Count Status	Accepted
Holiday	No
Start Date	Tue 8/8/2023
End Date	Wed 8/9/2023
Start Time	9:00:00 AM
End Time	9:00:00 AM
Direction	2-WAY
Station	
Study	
Speed Limit	
Description	
Sensor Type	NA
Source	CombineVolumeCountsIncremental
Latitude,Longitude	

INTERVAL:60-MIN	
Time	Hourly Count
0:00-1:00	11
1:00-2:00	6
2:00-3:00	5
3:00-4:00	7
4:00-5:00	19
5:00-6:00	49
6:00-7:00	100
7:00-8:00	223
8:00-9:00	195
9:00-10:00	189
10:00-11:00	200
11:00-12:00	229
12:00-13:00	225
13:00-14:00	272
14:00-15:00	268
15:00-16:00	286
16:00-17:00	290
17:00-18:00	320
18:00-19:00	219
19:00-20:00	159
20:00-21:00	144
21:00-22:00	81
22:00-23:00	44
23:00-24:00	32
<b>Total</b>	<b>3,573</b>
<b>AM Peak</b>	11:00-12:00 229
<b>PM Peak</b>	17:00-18:00 320

NOTES/FILES			
	Note	Date	



ITE PARKING GENERATION MANUAL, 6TH EDITION EXCERPTS

# Multifamily Housing - 2+ BR (Low-Rise) Not Close to Rail Transit (220)

Peak Period Parking Demand vs: Dwelling Units

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

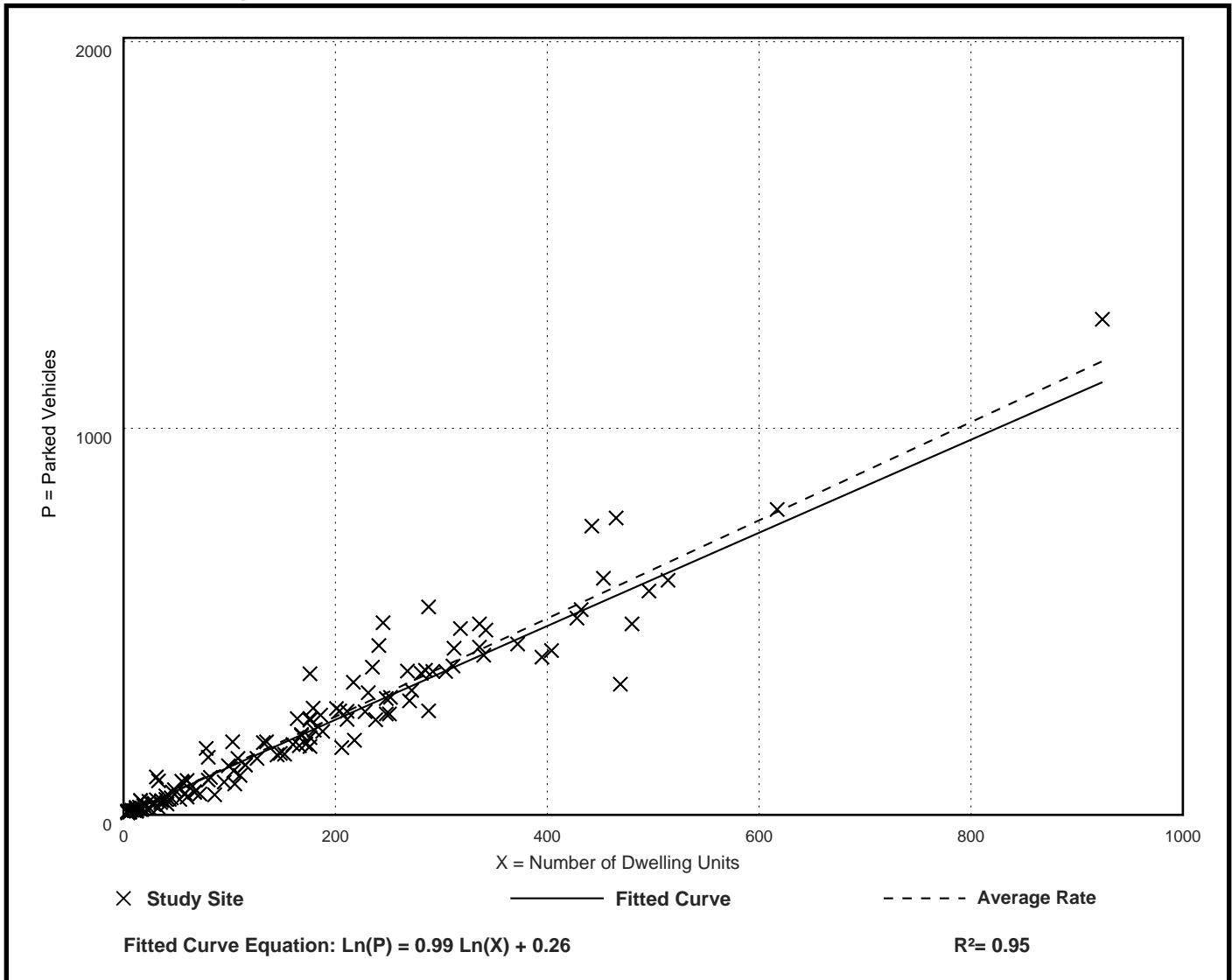
Number of Studies: 143

Avg. Num. of Dwelling Units: 154

## Peak Period Parking Demand per Dwelling Unit

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
1.27	0.58 - 3.16	1.07 / 1.59	1.22 - 1.32	0.29 ( 23% )

## Data Plot and Equation



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: 1000 Sq. Ft. GFA

On a: Weekday (Monday - Thursday)

Setting/Location: General Urban/Suburban

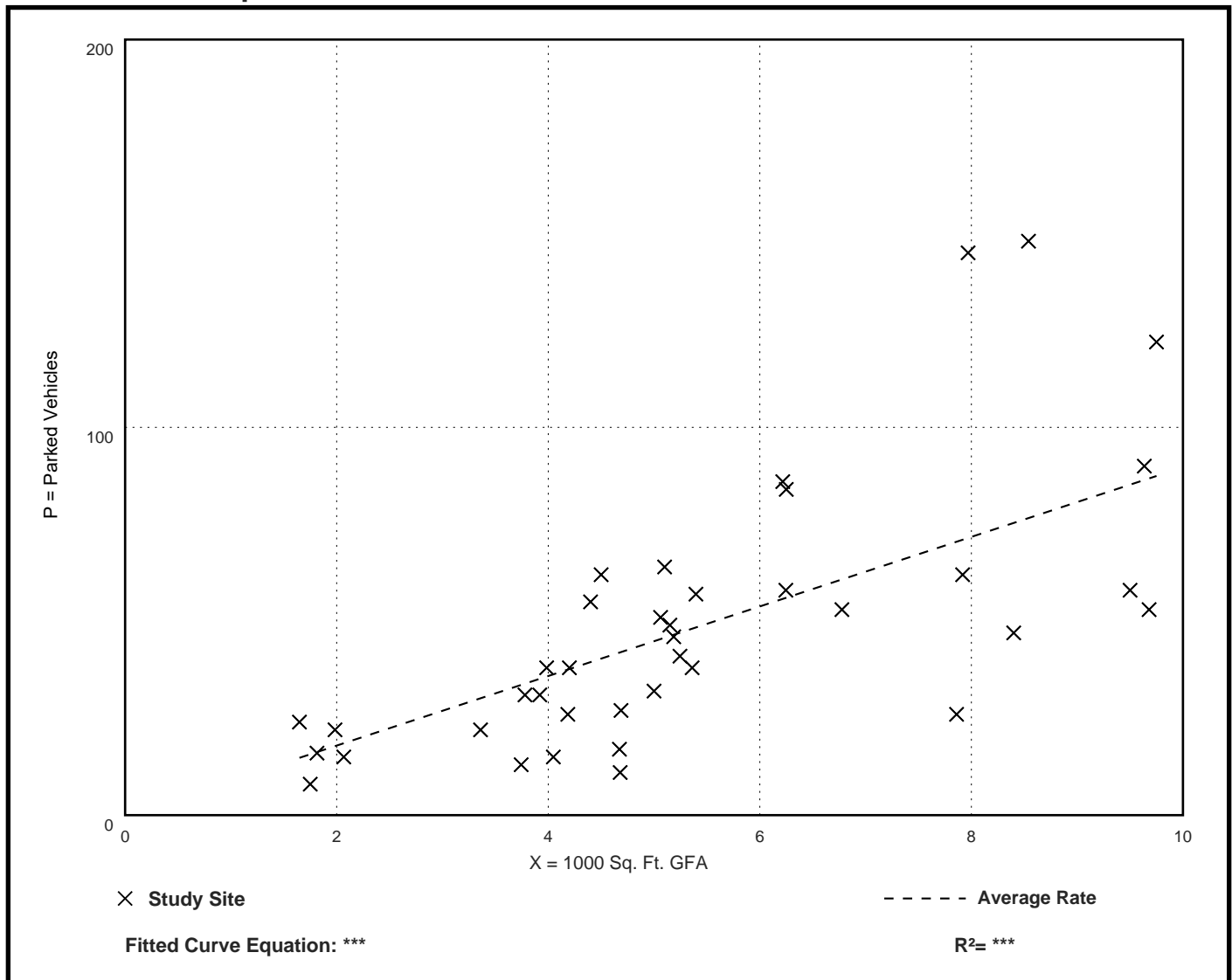
Number of Studies: 39

Avg. 1000 Sq. Ft. GFA: 5.4

## Peak Period Parking Demand per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
8.97	2.35 - 18.20	6.66 / 13.44	7.71 - 10.23	4.03 ( 45% )

## Data Plot and Equation



# Coffee/Donut Shop without Drive-Through Window (936)

Peak Period Parking Demand vs: 1000 Sq. Ft. GFA

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

Number of Studies: 12

Avg. 1000 Sq. Ft. GFA: 1.8

## Peak Period Parking Demand per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
10.36	3.49 - 19.31	8.15 / 16.80	***	4.84 ( 47% )

## Data Plot and Equation

